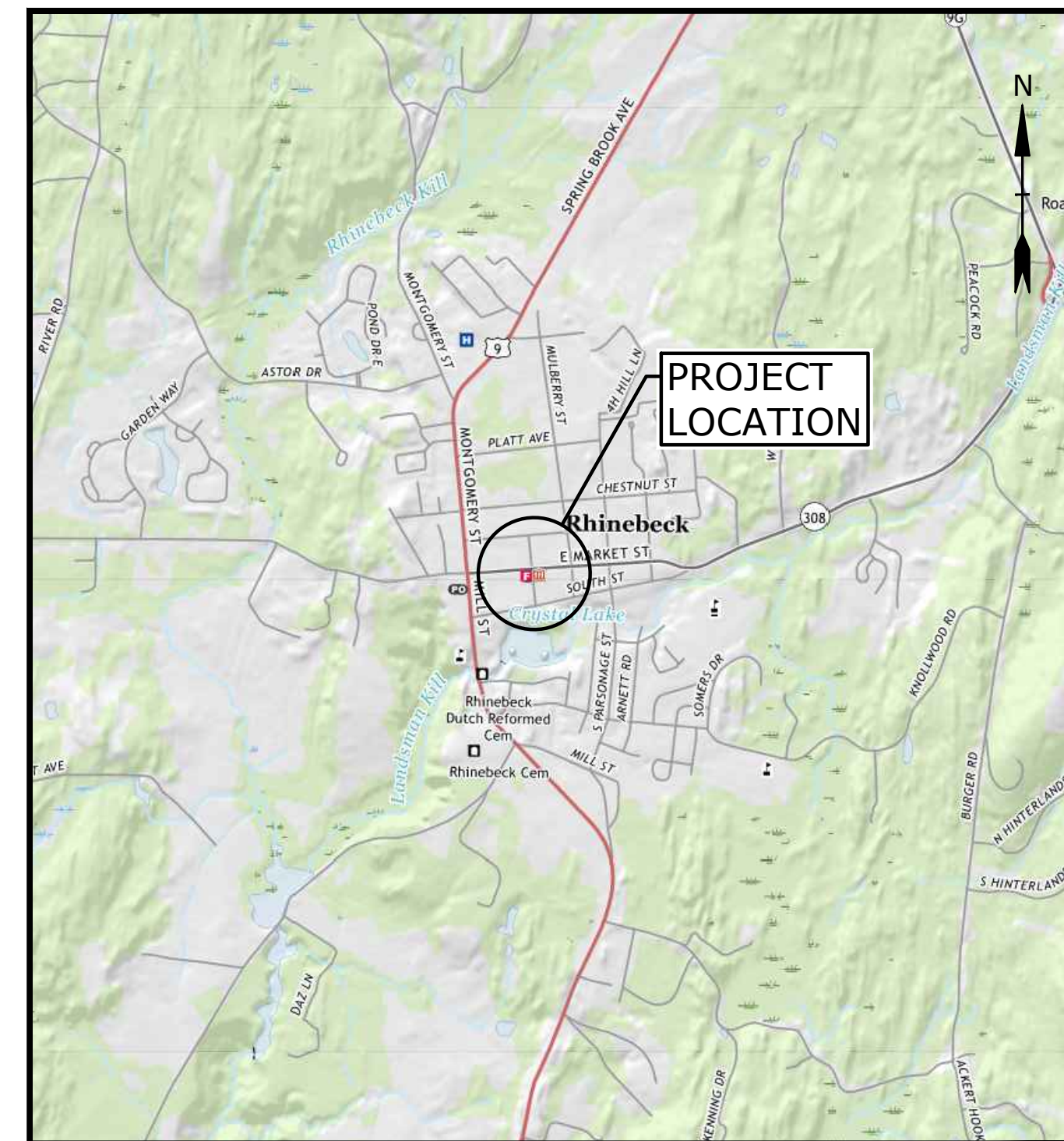


VILLAGE OF RHINEBECK, NY VILLAGE ACCESSIBILITY IMPROVEMENTS

ISSUED FOR CONSTRUCTION OCTOBER 2023

LIST OF DRAWINGS		
SHEET NO.	DRAWING NO.	DRAWING TITLE
1	G-001	COVER SHEET
2	G-002	GENERAL NOTES, LEGEND & ABBREVIATIONS
3	C-100	EXISTING CONDITIONS & DEMOLITION PLAN
4	C-101	SITE PLAN - CENTER STREET & VILLAGE HALL
5	C-102	SITE PLAN - MUNICIPAL PARKING LOT & FIRE DEPARTMENT
6	C-201	GRADING PLAN
7	C-501	SITE DETAILS - 1
8	C-502	SITE DETAILS - 2
9	C-503	SITE DETAILS - 3
10	C-504	SITE DETAILS - 4
11	C-505	SITE DETAILS - 5
NYS DOT STANDARD SHEETS FOR WORK ZONE TRAFFIC CONTROL		
12	619-010	WORK ZONE TRAFFIC CONTROL GENERAL NOTES
13	619-011	WORK ZONE TRAFFIC CONTROL GENERAL TABLES AND LEGEND
14-15	619-322	WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY ROADWAY CROSSWALK CLOSURE AND PEDESTRIAN DETOUR SHORT TERM OPERATIONS
16-17	619-410	WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY SHOULDER CLOSURE INTERMEDIATE TERM OPERATION
18-19	619-421	WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY ROADWAY FLAGGING OPERATION AT INTERSECTION INTERMEDIATE TERM OPERATION
20-21	619-519	WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY ROADWAY SIDEWALK DETOUR/DIVERSION LONG TERM OPERATION
22-23	619-520	WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY ROADWAY CROSSWALK CLOSURE AND PEDESTRIAN DETOUR LONG TERM OPERATION

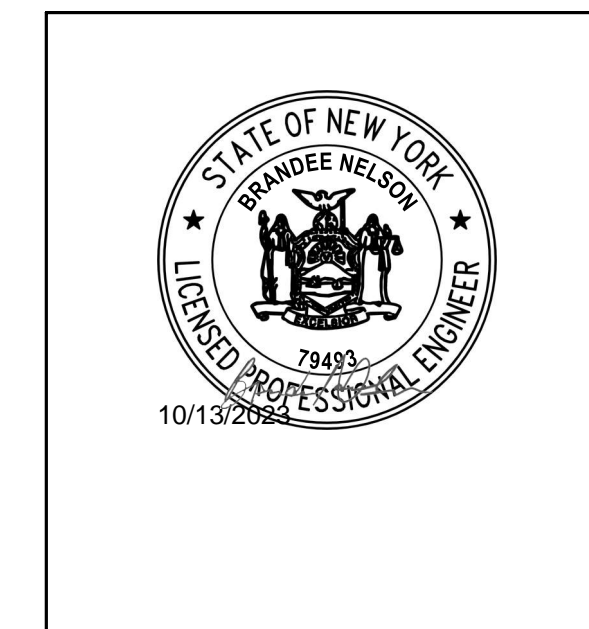


LOCATION MAP
SCALE: 1" = 2000'

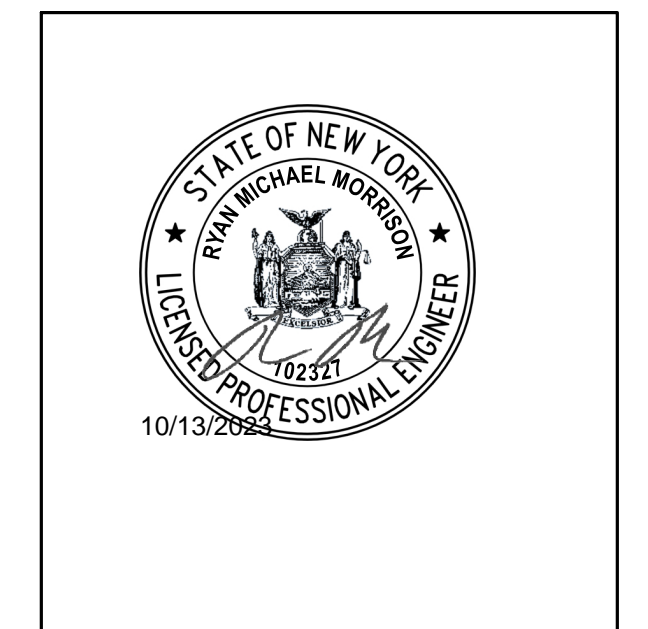
SOURCE: 2019 U.S.G.S. 7.5' TOPOGRAPHIC QUADRANGLE
KINGSTON EAST, NY

PREPARED BY:

Tighe & Bond



BRANEE NELSON, PE, LEED AP



RYAN MORRISON, PE

PREPARED FOR:

VILLAGE OF RHINEBECK
76 EAST MARKET STREET
RHINEBECK, NY

COMPLETE SET 23 SHEETS

LEGEND

DESCRIPTION	EXISTING	PROPOSED
PROPERTY LINE	-----	-----
RIGHT-OF-WAY LINE	-----	-----
LIMITS OF WORK	-----	-----
INTERMEDIATE CONTOURS	-----	-----
INDEX CONTOURS	----- 25 -----	----- 25 -----
SPOT GRADE	X 141.2	+ 32.0
MAGNITUDE & DIRECTION OF SLOPE		← 0.0%
STORM DRAIN	=====	=====
WATER SERVICE	----- W -----	----- W -----
OVERHEAD ELECTRIC	----- OE -----	----- OE -----
CURB	=====	=====
EDGE OF PAVEMENT	=====	=====
SIDEWALK	-----	-----
RETAINING WALL	=====	=====
STONE WALL	-----	-----
FENCE - UNSPECIFIED	----- X -----	----- X -----
STORM DRAIN STRUCTURES	MANHOLE (M) CATCH BASIN (CB)	
SANITARY SEWER MANHOLE	(S)	
WATER SERVICE STRUCTURES	HYDRANT (H) MANHOLE (M) VALVE (V)	
ELECTRIC SERVICE STRUCTURES	UTILITY CO. POLE (P) MANHOLE (M) LIGHT (L)	
TREELINE	-----	-----
TREE	(E) EVERGREEN (D) DECIDUOUS	
HEAVY DUTY REINFORCED CONCRETE SIDEWALK		[Pattern]
REINFORCED CONCRETE SIDEWALK		[Pattern]
HEAVY DUTY WMA PAVEMENT		[Pattern]
REGULAR DUTY WMA PAVEMENT		[Pattern]

ABBREVIATIONS

BC	BITUMINOUS CURB	IP	IRON PIN
BIT	BITUMINOUS	L	LENGTH OF CURB
BL	BASELINE	LSA	LANDSCAPED AREA
BLDG	BUILDING	MAX	MAXIMUM
BC	BOTTOM OF CURB	MIN	MINIMUM
BR	BOTTOM OF RAMP	MISC	MISCELLANEOUS
BS	BOTTOM OF STEP	MON	MONUMENT
BW	BOTTOM OF WALL	N	NORTH
CB	CATCH BASIN	NITC	NOT IN THIS CONTRACT
CEM	CEMENT	N/F	NOW OR FORMERLY
CHGE	CENTRAL HUDSON GAS & ELECTRIC	NYSDOT	NEW YORK STATE DEPARTMENT OF TRANSPORTATION
CIP	CAST IRON PIPE	PVMT	PAVEMENT
CONC	CONCRETE	R	RADIUS
CMP	CORRUGATED METAL PIPE	REV	REVISION
E	EAST	ROW	RIGHT OF WAY
EG	EXISTING GRADE	R&D	REMOVE AND DISPOSE
EL/ELEV	ELEVATION	R&R	REMOVE AND RESET
EOP	EDGE OF PAVEMENT	S	SOUTH
EXIST	EXISTING	SF	SQUARE FOOT
HC	HANDICAP	STRM	STORM
WMA	WARM MIX ASPHALT	TC	TOP OF CURB
HYD	HYDRANT	TR	TOP OF RAMP
IN	INCHES	TS	TOP OF STEP
INV	INVERT	TYP	TYPICAL
		VCP	VITRIFIED CLAY PIPE
		W	WATER
		WG	WATER GATE
		WV	WATER VALVE

GENERAL NOTES

- NOTIFY UDIG NY AT 1-800-962-7962 AND OTHER UTILITY OWNERS IN THE AREA NOT ON THE UDIG NY LIST AT LEAST 72 HOURS PRIOR TO ANY DIGGING, TRENCHING, ROCK REMOVAL, DEMOLITION, BORING, BACKFILLING, GRADING, LANDSCAPING, OR ANY OTHER EARTH MOVING OPERATIONS.
- LOCATIONS OF EXISTING UTILITIES ARE APPROXIMATE. IN ADDITION, SOME UTILITIES MAY NOT BE SHOWN. DETERMINE THE EXACT LOCATION OF UTILITIES BY TEST PIT OR OTHER METHODS, AS NECESSARY TO PREVENT DAMAGE TO UTILITIES AND/OR INTERRUPTIONS IN UTILITY SERVICE. PERFORM TEST PIT EXCAVATIONS AND OTHER INVESTIGATIONS TO LOCATE UTILITIES, AND PROVIDE THIS INFORMATION TO THE ENGINEER, PRIOR TO CONSTRUCTING THE PROPOSED IMPROVEMENTS. LOCATE ALL EXISTING UTILITIES TO BE CROSSED BY HAND EXCAVATION.
- NOT ALL OF THE UTILITY SERVICES TO BUILDINGS ARE SHOWN. THE CONTRACTOR SHALL ANTICIPATE THAT EACH PROPERTY HAS SERVICE CONNECTIONS FOR THE VARIOUS UTILITIES.
- BOLD TEXT AND LINES INDICATE PROPOSED WORK. LIGHT TEXT AND LINES INDICATE APPROXIMATE EXISTING CONDITIONS.
- TIGHE & BOND ASSUMES NO RESPONSIBILITY FOR ANY ISSUES, LEGAL OR OTHERWISE, RESULTING FROM CHANGES MADE TO THESE DRAWINGS WITHOUT WRITTEN AUTHORIZATION FROM TIGHE & BOND.
- EXCAVATE ADDITIONAL TEST PITS TO LOCATE EXISTING UTILITIES AS DIRECTED OR APPROVED BY THE ENGINEER.
- NOTIFY THE ENGINEER OF ANY UTILITIES IDENTIFIED DURING CONSTRUCTION THAT ARE NOT SHOWN ON THE DRAWINGS OR THAT DIFFER IN SIZE OR MATERIAL.
 - THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING AND COMPLYING WITH NYS DOT PERM 33.
- THE CONTRACTOR IS RESPONSIBLE FOR SITE SAFETY; COORDINATION WITH THE OWNER, ALL SUBCONTRACTORS, AND WITH OTHER CONTRACTORS WORKING WITHIN THE LIMITS OF WORK, THE MEANS AND METHODS OF CONSTRUCTING THE PROPOSED WORK.
- OBTAIN, PAY FOR AND COMPLY WITH PERMITS, NOTICES AND FEES NECESSARY TO COMPLETE THE WORK. ARRANGE AND PAY FOR NECESSARY INSPECTIONS AND APPROVALS FROM THE JURISDICTIONAL AUTHORITIES.
 - THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING AND COMPLYING WITH NYS DOT PERM 33.
- FIELD VERIFY ALL EXISTING CONDITIONS PRIOR TO CONSTRUCTION. IF FIELD CONDITIONS ARE OBSERVED THAT VARY SIGNIFICANTLY FROM THOSE SHOWN ON THE DRAWINGS, IMMEDIATELY NOTIFY THE ENGINEER IN WRITING FOR RESOLUTION OF THE CONFLICTING INFORMATION.
- PROTECT AND MAINTAIN ALL UTILITIES IN THE AREAS UNDER CONSTRUCTION DURING THE WORK. LEAVE ALL PIPES AND STRUCTURES WITHIN THE LIMITS OF THE CONTRACT IN A CLEAN AND OPERABLE CONDITION AT THE COMPLETION OF THE WORK. TAKE ALL NECESSARY PRECAUTIONS TO PREVENT SAND AND SILT FROM DISTURBED AREAS FROM ENTERING THE DRAINAGE SYSTEM.
- NOTIFY THE ENGINEER IN WRITING OF ANY CONFLICT, ERROR, AMBIGUITY, OR DISCREPANCY WITH THE PLANS OR BETWEEN THE PLANS AND ANY APPLICABLE LAW, REGULATION, CODE, STANDARD SPECIFICATION, OR MANUFACTURER'S INSTRUCTIONS.
- THE CONTRACTOR IS RESPONSIBLE FOR SUPPORT OF EXISTING UTILITIES AND REPAIR OR REPLACEMENT COSTS OF UTILITIES DAMAGED DURING CONSTRUCTION, WHETHER ABOVE OR BELOW GRADE. REPLACE DAMAGED UTILITIES IMMEDIATELY AT NO ADDITIONAL COST TO THE OWNER AND AT NO COST TO THE PROPERTY OWNER.
- TAKE NECESSARY MEASURES AND PROVIDE CONTINUOUS BARRIERS OF SUFFICIENT TYPE, SIZE, AND STRENGTH TO PREVENT ACCESS TO ALL WORK AND STAGING AREAS AT THE COMPLETION OF EACH DAYS WORK.
- NO OPEN TRENCHES WILL BE ALLOWED OVER NIGHT. THE USE OF ROAD PLATES TO PROTECT THE EXCAVATION WILL BE CONSIDERED UPON REQUEST, BUT BACKFILLING IS PREFERRED.
- THE CONTRACTOR IS RESPONSIBLE FOR ALL NECESSARY TRAFFIC CONTROL/SAFETY DEVICES TO ENSURE SAFE VEHICULAR AND PEDESTRIAN ACCESS THROUGH THE WORK AREA, OR FOR SAFELY IMPLEMENTING DETOURS AROUND THE WORK AREA. PERFORM TRAFFIC CONTROL IN ACCORDANCE WITH THE CONTRACTOR'S APPROVED TRAFFIC CONTROL PLAN.
 - COORDINATE WITH VILLAGE OF RHINEBECK FIRE CHIEF (BRYANT KNAPP, CHIEFKNAPP@VILLAGEOFRHINEBECKNY.GOV, 845-876-3133) FOR ALL WORK OCCURRING IN FRONT OF THE RHINEBECK FIRE DEPARTMENT.
- WHEN WORKING IN THE ROAD, PROVIDE THE OWNER AND LOCAL FIRE/POLICE/SCHOOL AUTHORITIES A DETAILED PLAN OF APPROACH INDICATING METHODS OF PROPOSED TRAFFIC ROUTING ON A DAILY BASIS. PROVIDE COORDINATION TO ENSURE COMMUNICATION AND COORDINATION BETWEEN THE OWNER, CONTRACTOR AND LOCAL FIRE/POLICE/SCHOOL AUTHORITIES THROUGHOUT THE CONSTRUCTION PERIOD.
- REMOVE AND DISPOSE OF ALL CONSTRUCTION-RELATED WASTE MATERIALS AND DEBRIS IN STRICT ACCORDANCE WITH ALL APPLICABLE LOCAL, STATE, AND FEDERAL LAWS.
- THE TERM "DEMOLISH" USED ON THE DRAWINGS MEANS TO REMOVE AND DISPOSE OF IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL REQUIREMENTS.
- THE TERM "ABANDON" USED ON THE DRAWINGS MEANS TO LEAVE IN PLACE AND TAKE APPROPRIATE MEASURES TO DECOMMISSION AS SPECIFIED OR NOTED ON THE DRAWINGS.
- ALL PROPOSED WORK MAY BE ADJUSTED IN THE FIELD BY THE OWNER'S PROJECT REPRESENTATIVE TO MEET EXISTING CONDITIONS.
- TEMPORARY ADA PEDESTRIAN ACCESS SHALL BE PROVIDED THROUGHOUT CONSTRUCTION ALONG THE STATE HIGHWAY OR APPROPRIATE PEDESTRIAN DETOURS PROVIDED PER NYS DOT STANDARDS. SEE SHEET 619.322-NYS DOT WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY ROADWAY CROSSWALK CLOSURE AND PEDESTRIAN DETOUR SHORT TERM OPERATIONS SHEETS PROVIDED IN PLAN SET.
- A SEPARATE CONTRACT HAS BEEN AWARDED FOR WORK WITHIN CENTER STREET INCLUDING REPLACEMENT OF THE WATER LINE AND REPAVING OF THE VILLAGE OWNED STREET. SCOPE OF WORK IS TO BE COORDINATED WITH OTHER CONTRACTOR INCLUDING BUT NOT LIMITED TO TRAFFIC CONTROL, PAVING SCHEDULE AND DISTURBANCE OF COMPLETED ITEMS.

EROSION CONTROL AND RESOURCE AREA PROTECTION NOTES

- PROVIDE ALL EROSION CONTROL MEASURES SHOWN, SPECIFIED, REQUIRED BY PERMIT, AND/OR REQUIRED BY THE ENGINEER PRIOR TO ANY CONSTRUCTION OR IMMEDIATELY UPON REQUEST. MAINTAIN SUCH CONTROL MEASURES UNTIL FINAL SURFACE TREATMENTS ARE IN PLACE AND/OR UNTIL PERMANENT VEGETATION IS ESTABLISHED. INSPECT AFTER EACH RAINSTORM AND DURING MAJOR STORM EVENTS TO CONFIRM THAT ALL SEDIMENTATION AND EROSION CONTROL MEASURES REQUIRED ARE IN PLACE AND EFFECTIVE.
- INSTALL SILT SACKS OR OTHER APPROVED SEDIMENTATION BARRIERS IN/AT ALL CATCH BASINS IN THE PROJECT AREA AS WELL AS CATCH BASINS/CURB INLETS THAT MAY RECEIVE RUNOFF FROM CONSTRUCTION ACTIVITIES. INLET PROTECTION BARRIERS MUST BE MAINTAINED FOR THE DURATION OF THE PROJECT. INLET PROTECTION BARRIERS MUST BE "HIGH FLOW SILT SACK" BY ACF ENVIRONMENTAL OR APPROVED EQUAL. INSPECT BARRIERS WEEKLY AND AFTER EACH STORM EVENT OF 0.50 INCHES OR GREATER. COMPLETE A MAINTENANCE INSPECTION REPORT AFTER EACH INSPECTION. SEDIMENT DEPOSITS MUST BE REMOVED AFTER EACH STORM EVENT OR MORE OFTEN IF THE FABRIC BECOMES CLOGGED OR SEDIMENT HAS ACCUMULATED TO 1/3 THE DESIGN DEPTH OF THE BARRIER.
- COMPACT, STABILIZE, AND LOAM AND SEED SIDE SLOPES, SHOULDER AREAS AND DISTURBED VEGETATED AREAS IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND AS REQUIRED BY PERMITS. GRADE SIDE SLOPES, SHOULDER AREAS AND DISTURBED VEGETATED AREAS TO A MAXIMUM SLOPE OF 3 HORIZONTAL TO 1 VERTICAL (3H:1V), WHERE POSSIBLE. PROVIDE BIODEGRADABLE EROSION CONTROL BLANKETS TO PREVENT EROSION WHERE SLOPES ARE STEEPER THAN 3H:1V.
- SETTLE OR FILTER ALL SILT-LADEN WATER FROM DEWATERING ACTIVITIES IN A SEDIMENTATION OR FILTER BAG TO REMOVE SEDIMENTS PRIOR TO RELEASE USING A SEDIMENTATION OR FILTER BAG LOCATED DOWN-GRADIENT OF THE DEWATERED AREA.
- REMOVE AND PROPERLY DISPOSE OF SILT TRAPPED AT BARRIERS IN UPLAND AREAS OUTSIDE BUFFER ZONES. REMOVE MATERIALS DEPOSITED IN ANY TEMPORARY SETTLING BASINS AT THE COMPLETION OF THE PROJECT. RESTORE ALL DISTURBED AREAS TO THEIR PRECONSTRUCTION CONDITION.
- SWEEP, COLLECT, REMOVE AND DISPOSE OF ANY SEDIMENT TRACKED ONTO PUBLIC RIGHT-OF-WAYS AT THE END OF EACH DAY.
- LOAM AND SEED ALL DISTURBED VEGETATED AREAS TO ESTABLISH COVER AND STABILIZATION AS SOON AS POSSIBLE FOLLOWING DISTURBANCE.
- MAINTAIN AN ADDITIONAL SUPPLY OF EROSION CONTROL MEASURES ON-SITE FOR EMERGENCY REPAIRS.
- STORE FUEL, OIL, PAINT, OR OTHER HAZARDOUS MATERIALS IN A SECONDARY CONTAINER AND REMOVE TO A SECURE LOCKED AND COVERED AREA DURING NON-WORK HOURS.
- PROVIDE A SUPPLY OF ABSORBENT SPILL RESPONSE MATERIALS SUCH AS BOOMS, BLANKETS, AND OIL ABSORBENT MATERIALS AT THE CONSTRUCTION SITE AT ALL TIMES TO CLEAN UP POTENTIAL SPILLS OF HAZARDOUS MATERIALS. IMMEDIATELY REPORT SPILLS OF HAZARDOUS MATERIALS TO THE STATE ENVIRONMENTAL AGENCY AND THE MUNICIPALITY WHERE THE WORK IS OCCURRING.

SURFACE RESTORATION NOTES

- ALL PAVEMENT DAMAGED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPLACED IN ACCORDANCE WITH THE CONTRACT DOCUMENTS.
 - ACCESSIBLE PARKING STALL AND PASSENGER LOADING ZONE (ANY DIRECTION) SLOPE < 2.0%
 - LONGITUDINAL SLOPE ALONG ACCESSIBLE ROUTES < 5.0%
 - CROSS SLOPE ALONG ACCESSIBLE ROUTES < 2.0%
- PROTECT PROJECT FEATURES (E.G., WALLS, FENCES, MAIL BOXES, SIGNS, SIDEWALKS, CURBING, STAIRS, WALKWAYS, TREES, ETC.) FROM DAMAGE DURING CONSTRUCTION, INCLUDING PROVIDING TEMPORARY SUPPORTS, WHEN APPROPRIATE.
- IF REMOVAL OF PROJECT FEATURES IS REQUIRED IN ORDER TO PERFORM THE PROPOSED WORK, REMOVE THOSE SITE FEATURES ONLY UPON APPROVAL OF ENGINEER. REPLACE ALL REMOVED PROJECT FEATURES; NEW ITEMS SHALL BE EQUAL OR BETTER IN QUALITY AND CONDITION TO THE ITEMS REMOVED.
- EXISTING SURVEY MONUMENTS DISTURBED BY THE CONTRACTOR SHALL BE REPLACED BY A LAND SURVEYOR LICENSED IN THE STATE IN WHICH THE WORK IS PERFORMED AT NO ADDITIONAL COST TO THE OWNER.
- COORDINATE THE ADJUSTMENT OF EXISTING UTILITY STRUCTURES WITH EACH RESPONSIBLE UTILITY OWNER PRIOR TO RECONSTRUCTION AND/OR PAVING OPERATIONS. RAISE ALL STRUCTURES TO FINISHED GRADES PRIOR TO THE END OF THE CONSTRUCTION SEASON AND PRIOR TO FINISHED PAVING.
- REPAIR DISTURBED PAVED SURFACES AT THE END OF EACH WORK WEEK, UNLESS OTHERWISE APPROVED/REQUIRED BY THE OWNER.
- PLACE TEMPORARY BITUMINOUS CONCRETE PAVEMENT AT DISTURBED PORTLAND CEMENT CONCRETE SIDEWALKS AND DRIVEWAYS AT THE END OF EACH WORK WEEK, UNLESS OTHERWISE APPROVED/REQUIRED BY THE OWNER.
- TRANSFER ALL TEMPORARY BENCHMARKS, AS NECESSARY.
- ACCOMMODATE PEDESTRIAN TRAFFIC WHERE A SIDEWALK IS TO BE CLOSED FOR SAFETY. "SIDEWALK CLOSED HERE" SIGNS SHALL BE USED AT THE NEAREST SAFE INTERSECTION. SEE TRAFFIC CONTROL DETAILS FOR SIGN INFORMATION.
- RESTORE ALL AREAS DISTURBED BY THE CONTRACTOR BEYOND THE PAYLINE LIMITS TO ORIGINAL CONDITIONS AT NO ADDITIONAL COST TO THE OWNER.
- REGRADE ALL UNPAVED AREAS DISTURBED BY THE WORK TO ORIGINAL CONTOURS OR PROPOSED CONTOURS AS REQUIRED BY THE DRAWINGS. TOPSOIL AND SEED ALL UNPAVED AREAS DISTURBED BY THE WORK. OVER-EXCAVATE UNPAVED AREAS AS REQUIRED TO PROVIDE REQUIRED TOPSOIL DEPTH AND MEET ADJACENT GRADE.
- REPAIR/REPLACE PAVED SURFACES DISTURBED BY THE WORK IN-KIND, UNLESS OTHERWISE NOTED. RESTORE SURFACES TO EXISTING OR PROPOSED CONDITIONS AS INDICATED ON THE DRAWINGS.
- PROVIDE A SMOOTH, FLUSH TRANSITION BETWEEN ALL NEW AND EXISTING PAVEMENTS AND WALKING SURFACES

Tighe & Bond



ISSUED FOR CONSTRUCTION

Village Accessibility Improvements

Village of Rhinebeck

Rhinebeck, NY

SERVICES PROVIDED IN NEW YORK BY T&B ENGINEERING AND LANDSCAPE ARCHITECTURE, P.C.

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2	10/12/2023	ISSUED FOR CONSTRUCTION
1	9/22/2023	ADDENDUM #1
MARK	DATE	DESCRIPTION
PROJECT NO: R5009-032		
DATE: 09/05/2023		
FILE: R5009-032-ADA IMPROVEMENTS PLAN SET.dwg		
DRAWN BY: HC / RQL		
DESIGNED/CHECKED BY: RMM		
APPROVED BY: BKN		

GENERAL NOTES, LEGEND & ABBREVIATIONS

SCALE: NO SCALE

G-002
SHEET 2 OF 23

Last Saved: 10/12/2023 11:15am By: R. Lewis
Plotted On: Oct 12, 2023 11:15am By: R. Lewis
Tighe & Bond | 315 R5009 Village of Rhinebeck 032 Village Accessibility Improvements | Drawings\AutoCAD\Sheet\032-ADA IMPROVEMENTS PLAN SET.dwg



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Village of Rhinebeck

Rhinebeck, NY

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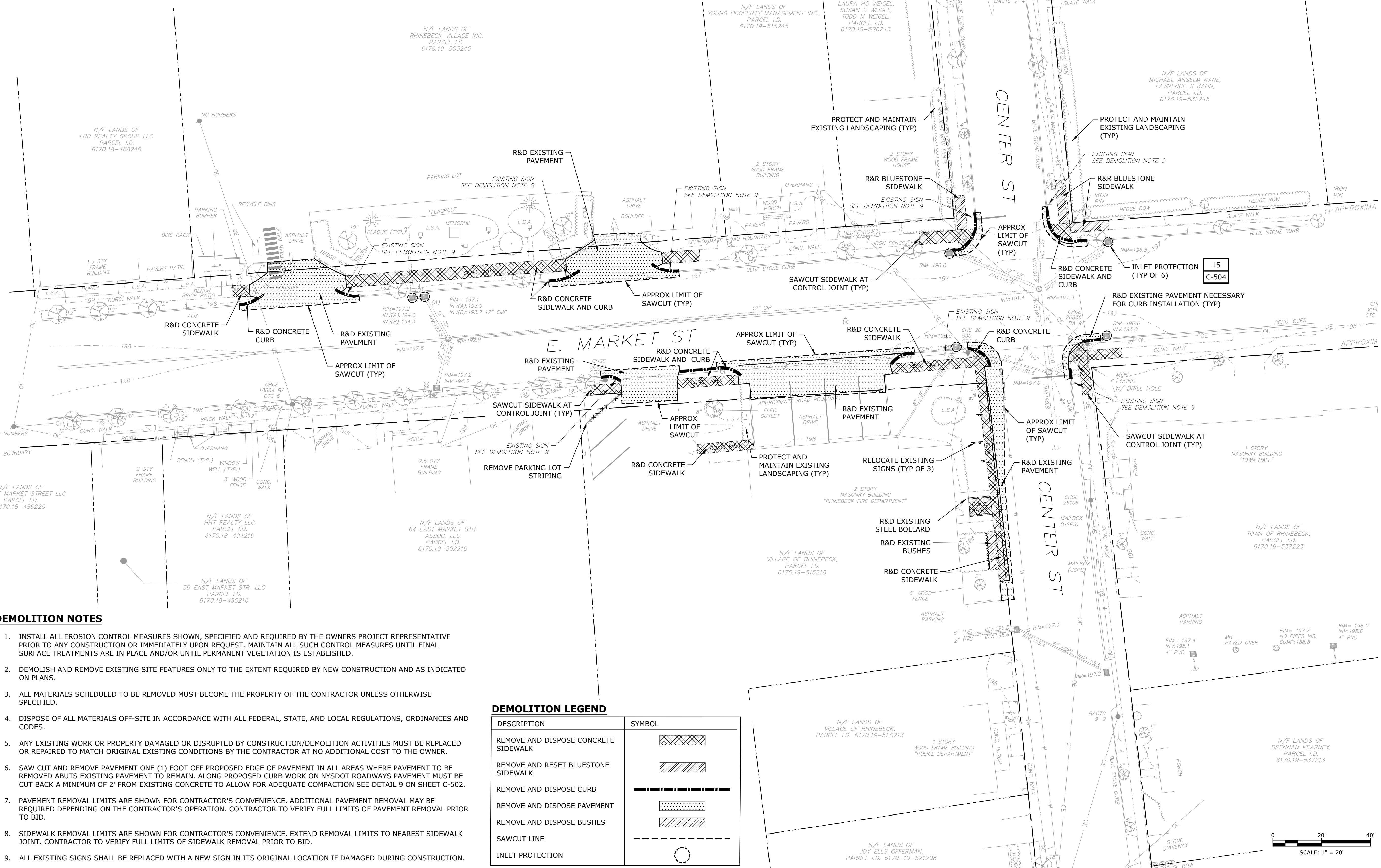
EXISTING CONDITIONS & DEMOLITION PLAN

SCALE: AS SHOWN

C-100
SHEET 3 OF 23

BASE PLAN NOTES

- THE EXISTING CONDITIONS INFORMATION SHOWN ON THE DRAWINGS IS BASED ON THE FOLLOWING:
 - SURVEY DRAWINGS PROVIDED BY CONTROL POINT ASSOCIATES, INC. PC TITLED SURVEY EXHIBIT CENTER STREET AND DATED DECEMBER 5, 2022.
 - FIELD INVESTIGATIONS PERFORMED BY TIGHE & BOND ON DECEMBER 28, 2022
- THE DRAWINGS ARE BASED ON THE FOLLOWING DATUMS: HORIZONTAL-NAD83 ; VERTICAL-NAVD88
- THE PROPERTY LINES SHOWN ON THE DRAWINGS ARE APPROXIMATE AND ARE NOT BASED ON DEED OR PLAN RESEARCH.



DEMOLITION NOTES

- INSTALL ALL EROSION CONTROL MEASURES SHOWN, SPECIFIED AND REQUIRED BY THE OWNERS PROJECT REPRESENTATIVE PRIOR TO ANY CONSTRUCTION OR IMMEDIATELY UPON REQUEST. MAINTAIN ALL SUCH CONTROL MEASURES UNTIL FINAL SURFACE TREATMENTS ARE IN PLACE AND/OR UNTIL PERMANENT VEGETATION IS ESTABLISHED.
- DEMOLISH AND REMOVE EXISTING SITE FEATURES ONLY TO THE EXTENT REQUIRED BY NEW CONSTRUCTION AND AS INDICATED ON PLANS.
- ALL MATERIALS SCHEDULED TO BE REMOVED MUST BECOME THE PROPERTY OF THE CONTRACTOR UNLESS OTHERWISE SPECIFIED.
- DISPOSE OF ALL MATERIALS OFF-SITE IN ACCORDANCE WITH ALL FEDERAL, STATE, AND LOCAL REGULATIONS, ORDINANCES AND CODES.
- ANY EXISTING WORK OR PROPERTY DAMAGED OR DISRUPTED BY CONSTRUCTION/DEMOLITION ACTIVITIES MUST BE REPLACED OR REPAIRED TO MATCH ORIGINAL EXISTING CONDITIONS BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
- SAW CUT AND REMOVE PAVEMENT ONE (1) FOOT OFF PROPOSED EDGE OF PAVEMENT IN ALL AREAS WHERE PAVEMENT TO BE REMOVED ABUTS EXISTING PAVEMENT TO REMAIN. ALONG PROPOSED CURB WORK ON NYSOT ROADWAYS PAVEMENT MUST BE CUT BACK A MINIMUM OF 2' FROM EXISTING CONCRETE TO ALLOW FOR ADEQUATE COMPACTION SEE DETAIL 9 ON SHEET C-502.
- PAVEMENT REMOVAL LIMITS ARE SHOWN FOR CONTRACTOR'S CONVENIENCE. ADDITIONAL PAVEMENT REMOVAL MAY BE REQUIRED DEPENDING ON THE CONTRACTOR'S OPERATION. CONTRACTOR TO VERIFY FULL LIMITS OF PAVEMENT REMOVAL PRIOR TO BID.
- SIDEWALK REMOVAL LIMITS ARE SHOWN FOR CONTRACTOR'S CONVENIENCE. EXTEND REMOVAL LIMITS TO NEAREST SIDEWALK JOINT. CONTRACTOR TO VERIFY FULL LIMITS OF SIDEWALK REMOVAL PRIOR TO BID.
- ALL EXISTING SIGNS SHALL BE REPLACED WITH A NEW SIGN IN ITS ORIGINAL LOCATION IF DAMAGED DURING CONSTRUCTION.

DEMOLITION LEGEND

DESCRIPTION	SYMBOL
REMOVE AND DISPOSE CONCRETE SIDEWALK	[Cross-hatched pattern]
REMOVE AND RESET BLUESTONE SIDEWALK	[Diagonal hatched pattern]
REMOVE AND DISPOSE CURB	[Dashed line]
REMOVE AND DISPOSE PAVEMENT	[Dotted pattern]
REMOVE AND DISPOSE BUSHES	[Diagonal hatched pattern]
SAWCUT LINE	[Dashed line]
INLET PROTECTION	[Circle with cross]

Last Saved: 10/12/2023 4:36pm By: Rluwis
 Plotted On: Oct 12, 2023 4:36pm
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 Title: Village Accessibility Improvements Drawings AutoCAD/Sheet/032-ADA IMPROVEMENTS PLAN SET.dwg



ISSUED FOR CONSTRUCTION

Village Accessibility Improvements

Village of Rhinebeck

Rhinebeck, NY

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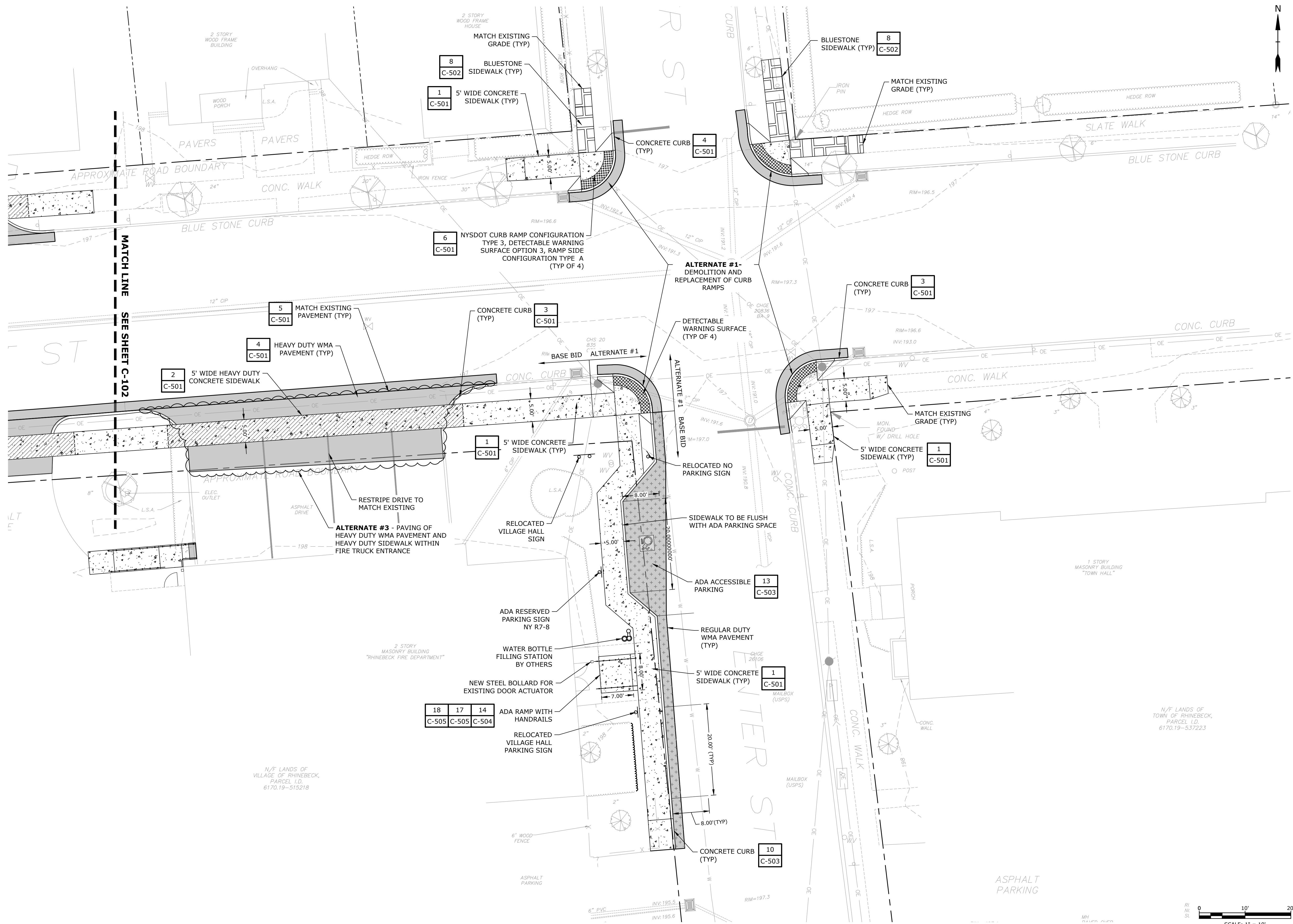
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SITE PLAN - CENTER STREET & VILLAGE HALL

SCALE: AS SHOWN

C-101
SHEET 4 OF 23



Last Saved: 10/12/2023 11:15am By: R. Lewis
 Plotted On: Oct 12, 2023 11:15am
 Tighe & Bond | R5009-032-Village Accessibility Improvements | Drawings\AutoCAD\Sheet\5009-032-ADA IMPROVEMENTS PLAN SET.dwg



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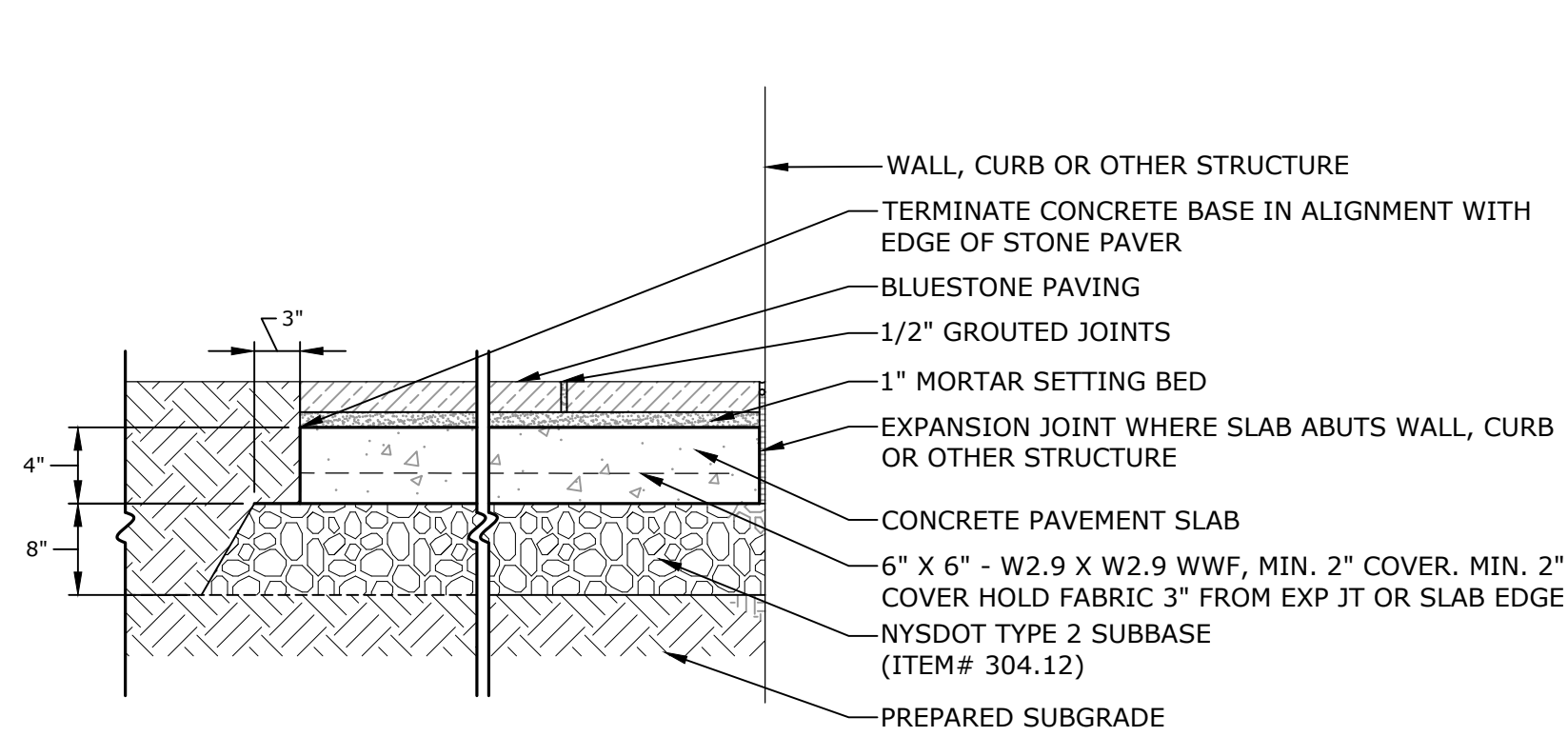
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APPROVED BY: BKN		

SITE DETAILS - 2

SCALE: NO SCALE

C-502
SHEET 8 OF 23

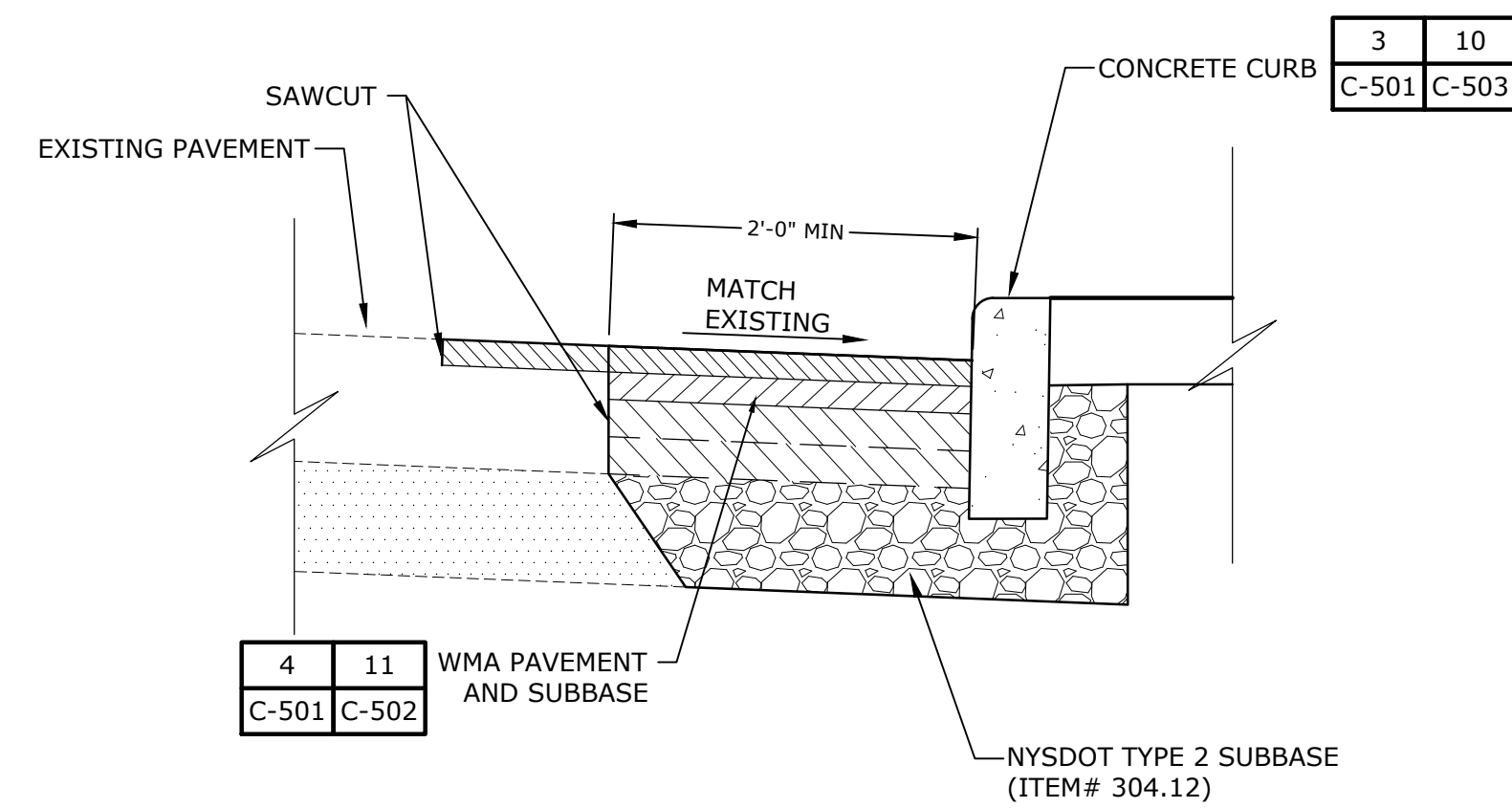


NOTES

1. PROVIDE STEEL DOWELS AT EXPANSION JOINTS INTERNAL TO SLAB AND WHERE ABUTTING ADJACENT CONCRETE PAVING OR SLAB BASE.
2. COORDINATE SLAB BASE EXPANSION JOINT LAYOUT TO ALIGN WITH EXPANSION JOINTS IN APPROVED STONE PAVER SHOP DRAWINGS.
4. SIDEWALK WORK TO OCCUR IN ACCORDANCE WITH NYSOT SPECIFICATION 608 FOR GROUDED STONE BLOCK PAVED SIDEWALKS AND DRIVEWAYS (MORTAR SETTING BED).
5. NYSOT ITEM NUMBER FOR SIDEWALKS IS 608.04

BLUESTONE PAVING (NYSOT)

DETAIL	8
NO SCALE	-



NOTES

1. WHERE EXISTING ROADWAYS ARE SAWCUT TO INSTALL CURBING AND/OR SIDEWALK, THE ROADWAY SHALL BE SAWCUT AT LEAST 2'-0" FROM THE PROPOSED CURB LINE TO ALLOW FOR ADEQUATE COMPACTION OF ASPHALT.

PAVEMENT RECONSTRUCTION ALONG CURB (NYSOT)

DETAIL	9
NO SCALE	-



ISSUED FOR CONSTRUCTION

Village Accessibility Improvements

Village of Rhinebeck

Rhinebeck, NY

SERVICES PROVIDED IN NEW YORK BY T&B ENGINEERING AND LANDSCAPE ARCHITECTURE, P.C.

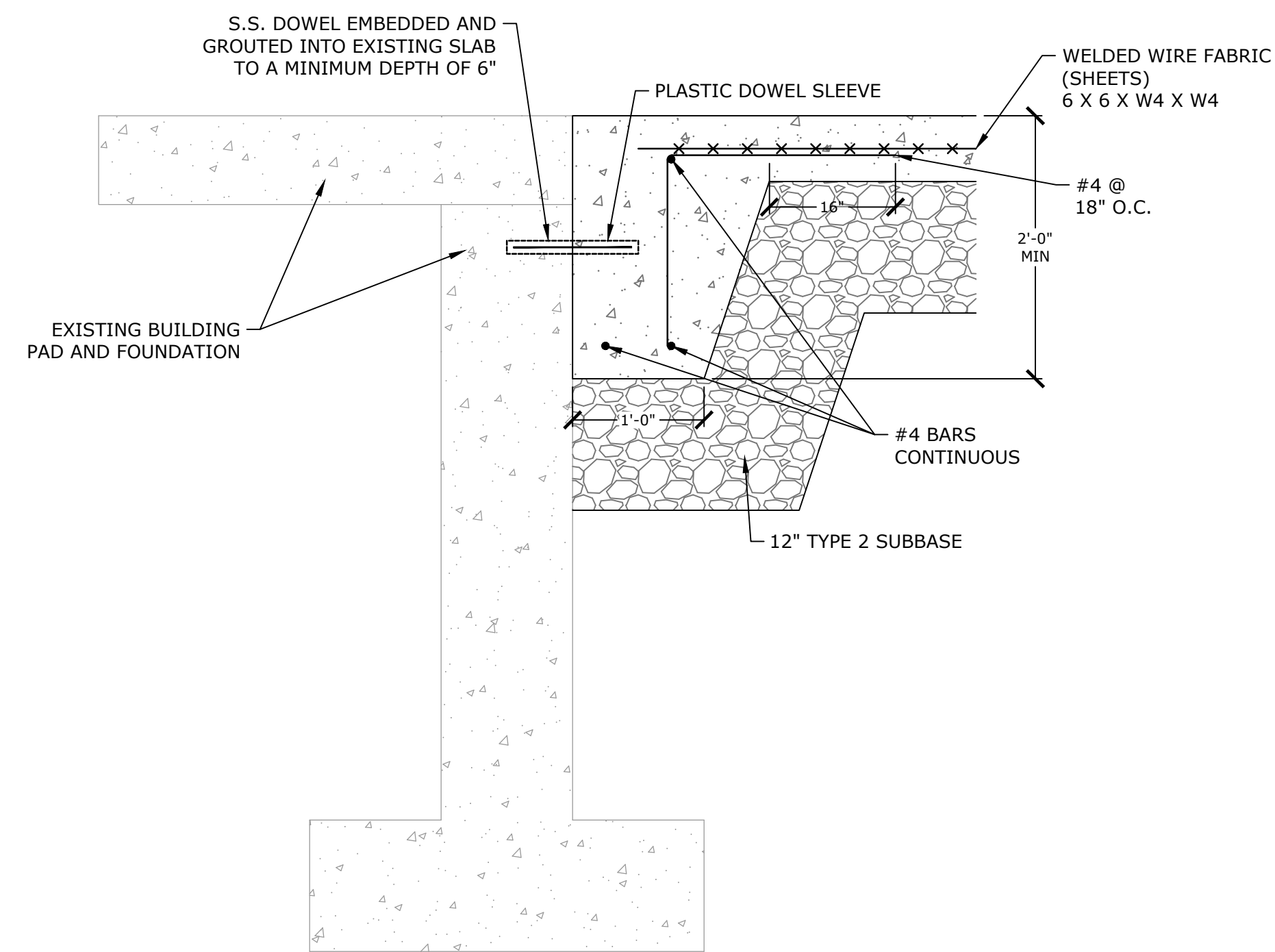
UNAUTHORIZED ALTERATIONS TO THIS DOCUMENT IS A VIOLATION OF SECTION 7209 OF THE NEW YORK STATE EDUCATION LAW.

2	10/12/2023	ISSUED FOR CONSTRUCTION
1	9/22/2023	ADDENDUM #1
MARK	DATE	DESCRIPTION
PROJECT NO:	R5009-032	
DATE:	09/05/2023	
FILE:	R5009-032-C-501.dwg	
DRAWN BY:	HC / RQL	
DESIGNED/CHECKED BY:	RMM	
APPROVED BY:	BKN	

SITE DETAILS - 5

SCALE: NO SCALE

C-505
SHEET 11 OF 23

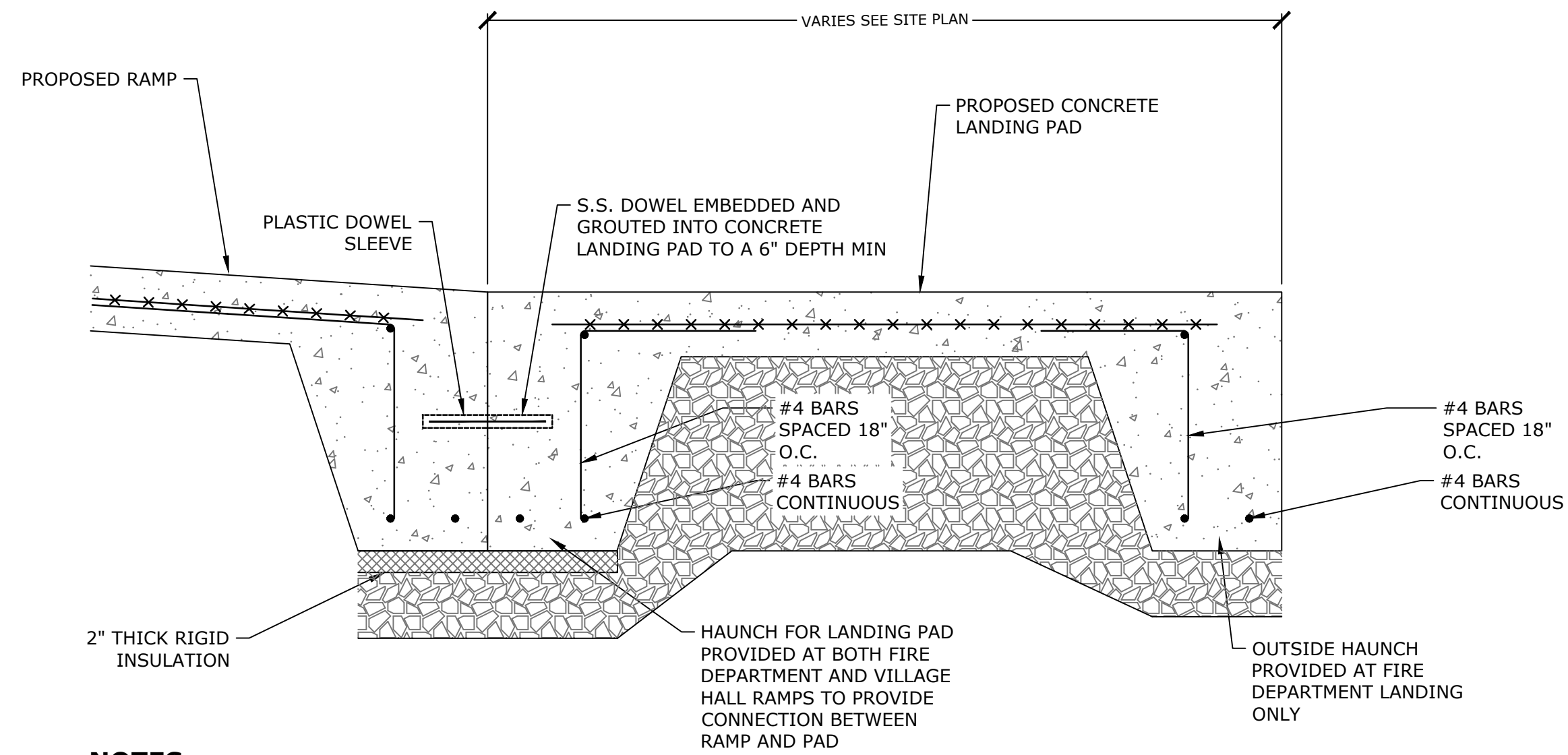


NOTES:

- CONTRACTOR IS TO TIE NEW ADA RAMP TO EXISTING BUILDING SLAB WITH #4 REBAR SPACED 12" O.C. #4 STAINLESS STEEL DOWEL TO EMBEDDED AND GROUTED INTO EXISTING SLAB TO A MINIMUM DEPTH OF 6". CONTRACTOR TO PROVIDE PLASTIC SLEEVE IN PROPOSED CONCRETE.
- CONTRACTOR TO PROVIDE EXPANSION JOINT BETWEEN EXISTING CONCRETE AND PROPOSED RAMP.

BUILDING PAD TO RAMP CONNECTION

DETAIL	17
NO SCALE	-



NOTES:

- CONTRACTOR IS TO TIE NEW ADA RAMP TO PROPOSED LANDING SLAB WITH #4 REBAR SPACED 12" O.C. #4 STAINLESS STEEL DOWEL TO A MINIMUM DEPTH OF 6".
- CONTRACTOR TO PROVIDE EXPANSION JOINT BETWEEN EXISTING CONCRETE AND PROPOSED RAMP.

RAMP TO LANDING PAD CONNECTION

DETAIL	18
NO SCALE	-

GENERAL NOTES

- 1. THE TYPICAL DETAILS DEPICTED ON THE STANDARD SHEETS AND IN THE MUTCD, REFLECT THE MINIMUM REQUIREMENTS.
2. PROPOSED REVISIONS TO THE TRAFFIC CONTROL PLAN SHALL BE PROVIDED, IN WRITING, TO THE DOT ENGINEER FOR REVIEW AND APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE FIVE (5) WORK DAYS PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH PROPOSED REVISIONS, EXCEPT FOR CHANGES THAT ALTER THE SCOPE OF THE TRAFFIC CONTROL PLAN. SUCH CHANGES IN SCOPE MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE THIRTY (30) WORK DAYS PRIOR TO IMPLEMENTATION OF SUCH REVISIONS.
3. THE NAMES, ADDRESSES, AND TELEPHONE NUMBERS OF STAFF WHO ARE AUTHORIZED TO SECURE LABOR, MATERIALS, AND EQUIPMENT FOR EMERGENCY REPAIRS OUTSIDE NORMAL WORKING HOURS SHALL BE PROVIDED, IN WRITING, TO THE NYSDOT ENGINEER. THE ENGINEER WILL PROVIDE THE SUBMITTED INFORMATION TO REGIONAL MANAGEMENT, THE NEW YORK STATE POLICE, THE RESIDENT ENGINEER, AND THE LOCAL POLICE.
4. STANDARD SHEET 619-503 MAY BE USED FOR AN OFFSITE DETOUR SETUP FOR BOTH LONG TERM AND SHORT TERM WORK DURATIONS.
5. REGIONAL HIGH-VOLUME RESTRICTIONS SHALL BE FOLLOWED. CONSULT WITH DOT ENGINEER IF EXCEPTION NEEDED.
6. PLAN AHEAD TO AVOID CONFLICTING WORK ZONES. CHECK FOR CONSTRUCTION PROJECTS, CLOSURES, & RESTRICTIONS AT WWW.511NY.ORG, WWW.DOT.NY.GOV/PROJECTS, AND WITH NYSDOT ENGINEER.
7. WORK ZONE INCIDENTS SHALL BE DOCUMENTED AND REPORTED USING EITHER THE DEPARTMENT'S WORK ZONE INCIDENT FORM, OR THE CONSTRUCTION INCIDENT REPORTING SYSTEM, AS APPROPRIATE.
8. CONSIDER CLOSURE WIDTH AND THE ABILITY TO ACCOMMODATE WIDE LOAD VEHICLES BEFORE ESTABLISHING WORK ZONES.
9. IF THE WORK ZONE AFFECTS AN EXISTING ACCESSIBLE AND DETECTABLE PEDESTRIAN FACILITY, ACCESSIBILITY AND DETECTABILITY SHALL BE PROVIDED ALONG THE ALTERNATE ROUTE.

ACTIVITY AREA

- 1. A 500' MINIMUM LONGITUDINAL DISTANCE SHALL BE MAINTAINED BETWEEN CONSTRUCTION OPERATIONS ON ALTERNATE SIDES OF THE ROADWAY, UNLESS OTHERWISE APPROVED BY THE ENGINEER.
2. WHEN TWO OR MORE AREAS ARE ADJACENT, OVERLAP, OR ARE IN CLOSE PROXIMITY, THE CONTRACTOR SHALL ENSURE THERE ARE NO CONFLICTING SIGNS AND THAT LANE CONTINUITY IS MAINTAINED THROUGHOUT ALL WORK AREAS.

SIGNS

- 1. THE LOCATIONS OF THE SIGNS SHOWN ON THE WORK ZONE TRAFFIC CONTROL PLANS AND DETAILS MAY BE ADJUSTED BASED ON SIGHT DISTANCE AND OTHER CONSIDERATIONS. THE FINAL LOCATIONS OF SIGNS ARE SUBJECT TO APPROVAL OF THE ENGINEER.
2. FOR LONG TERM WORK DURATIONS, ANY EXISTING SIGNS, INCLUDING OVERHEAD SIGNS, WHICH CONFLICT WITH THE TEMPORARY TRAFFIC CONTROL SIGN LAYOUT SHALL BE COVERED, REMOVED, STORED OR RESET, AS APPROVED BY THE ENGINEER. ALL APPROPRIATE EXISTING SIGNS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND/OR LOCATION UNLESS OTHERWISE REPLACED IN THIS CONTRACT.
3. SIGNS AT OR NEAR INTERSECTIONS SHALL BE PLACED SO THAT THEY DO NOT OBSTRUCT A MOTORIST'S LINE OF SIGHT.
4. SIGNS MOUNTED ON THE MEDIAN OF DIVIDED HIGHWAYS WHERE MEDIAN BARRIER IS IN PLACE MAY BE MOUNTED ON THE BARRIER WITH A SADDLE TYPE BRACKET OR OMITTED WITH THE APPROVAL OF THE DOT ENGINEER. LAYING THE SIGN DOWN IN A HORIZONTAL POSITION IS NOT PERMITTED.
5. THE DIMENSIONS OF WORK ZONE TRAFFIC CONTROL SIGNS ARE DESCRIBED IN THE MUTCD. ANY CHANGES TO THE DIMENSIONS SHALL BE APPROVED BY THE REGIONAL DIRECTOR OR BY HIS/HER DESIGNEE.
6. NYR9-12 SHALL BE USED IN PLACE OF NYR9-11 WHEN A REDUCED REGULATORY SPEED LIMIT SIGN IS AUTHORIZED.
7. RIGID AND FLEXIBLE "ROLL-UP" SIGNS MAY BE USED FOR MOBILE, SHORT DURATION AND SHORT-TERM STATIONARY WORK. RIGID SIGNS MUST BE MOUNTED AT LEAST 5 FEET ABOVE GRADE (7 FEET WHERE THERE ARE PEDESTRIANS OR PARKED CARS). FLEXIBLE SIGNS SHALL BE MOUNTED AT LEAST ONE FOOT ABOVE GRADE. MESH SIGNS SHALL NOT BE USED. USE RETRO REFLECTORIZED RIGID SIGNS FOR NIGHTTIME WORK.

CHANNELIZING DEVICES

- 1. WHERE POSSIBLE ALL CHANNELIZING AND GUIDING DEVICES ARE TO BE PLACED SO AS TO PROVIDE A MINIMUM 2' LATERAL CLEARANCE TO THE TRAVELED WAY.

PUBLIC ACCESS

- 1. PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE INACCESSIBLE SHALL BE NOTIFIED AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVEWAY. FOR MULTIPLE ACCESS PROPERTIES, AT LEAST ONE DRIVEWAY SHALL BE OPEN AT ALL TIMES. ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS POSSIBLE.
2. SUITABLE RAMPS SHALL BE INSTALLED TO MAINTAIN SMOOTH TRANSITIONS FROM RESIDENTIAL AND COMMERCIAL DRIVEWAYS TO AND FROM THE WORK AREA.

LANE CLOSURES

- 1. LANE CLOSURES SHALL BE LOCATED TO PROVIDE OPTIMUM VISIBILITY, I.E. BEFORE CURVES AND CRESTS, TO THE EXTENT CONDITIONS PERMIT.
2. THE ENGINEER MAY REQUIRE THAT ALL LANES BE RE-OPENED AT ANY TIME IF THE ROUTE IS NEEDED FOR EMERGENCY PURPOSES. THIS COULD INCLUDE INCIDENTS AT LOCATIONS OUTSIDE THE CONTRACT LIMITS.
3. EACH ARROW PANEL SHALL BE VISIBLE 1500 FEET IN ADVANCE FROM ANY POINT WITHIN THE ROADWAY.

LANE WIDTHS

- 1. UNLESS AUTHORIZED BY THE ENGINEER, THE MINIMUM LANE WIDTHS FOR WORK ZONE TRAVEL LANES SHALL BE AS FOLLOWS: FREEWAYS AND/OR EXPRESSWAYS IS 11'. THE MINIMUM LANE WIDTH FOR ALL OTHER TYPES OF ROADWAYS IS 10'.
2. A WRITTEN NOTE SHALL BE PROVIDED TO THE ENGINEER, A MINIMUM OF 21 CALENDAR DAYS IN ADVANCE OF PERFORMING ANY WORK THAT RESULTS IN THE REDUCED WIDTH OF AN EXISTING ROADWAY, SO THAT THE ENGINEER MAY NOTIFY THE REGIONAL PERMIT ENGINEER IN A TIMELY MANNER.
3. IF THE WORK ZONE AFFECTS PEDESTRIANS, A MINIMUM PEDESTRIAN PATHWAY WIDTH OF 5 FEET SHALL BE MAINTAINED UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.
4. TEMPORARY BICYCLE ACCOMMODATIONS SHALL NOT BE LESS THAN WHAT CURRENTLY EXISTS UNLESS AUTHORIZED BY THE ENGINEER.

PROTECTIVE VEHICLES

- 1. PROTECTIVE VEHICLES ARE DIVIDED INTO 2 CATEGORIES BASED ON THE GROSS VEHICLE WEIGHT (GVW):
- PROTECTIVE VEHICLE LIGHT (PVL) SHALL HAVE A MINIMUM GVW OF 9,500 LBS. OR GREATER.
- PROTECTIVE VEHICLE HEAVY (PVH) SHALL HAVE A MINIMUM GVW OF 22,000 LBS. OR GREATER.
2. IF THE PROTECTIVE VEHICLE ENCLOSED INTO THE TRAVEL LANE, OR IF IT REMAINS ENTIRELY ON THE SHOULDER OF ANY HIGH SPEED ROAD (>45 MPH), IT SHALL BE EQUIPPED WITH A DEPLOYED TRUCK/TRAILER MOUNTED IMPACT ATTENUATOR (TMIA, SEE TABLE 011-01 ON SHEET 619-011). BALLAST MAY BE USED TO BRING A LIGHTER VEHICLE UP TO THE INDICATED WEIGHT PROVIDED THE BALLAST IS SECURELY CONTAINED WITHIN AN ENCLOSED BODY OR OTHERWISE SECURELY FASTENED TO THE VEHICLE PURSUANT TO FEDERAL MOTOR CARRIER SAFETY ADMINISTRATION (FMCSA) CARGO SECUREMENT RULES, SUCH THAT:
1) THE BALLAST WILL NOT SEPARATE FROM THE VEHICLE UPON IMPACT AND
2) THE BALLAST WEIGHT WILL NOT EXCEED THE MANUFACTURER'S GROSS VEHICLE WEIGHT RATING (GVWR).
TRUCK/TRAILER MOUNTED IMPACT ATTENUATORS SHALL NOT BE MOUNTED/INSTALLED ON VEHICLES WITH A GROSS WEIGHT (GVW) LESS THAN WHAT IS MINIMALLY REQUIRED BY THE MANUFACTURER OF THE TMIA.
3. WHEN A PROTECTIVE VEHICLE(S) IS USED BETWEEN THE WORK VEHICLE (CREW) OR HAZARD AND THE TRAFFIC IN A MOVING OPERATION IT IS REFERRED TO AS A SHADOW VEHICLE(S).
4. WHEN A PROTECTIVE VEHICLE(S) IS USED BETWEEN THE WORK VEHICLE (CREW) OR HAZARD AND THE TRAFFIC IN A STATIONARY OPERATION IT IS REFERRED TO AS A BARRIER VEHICLE(S).
5. WHEN A PROTECTIVE VEHICLE IS USED IN ADVANCE OF EITHER MOVING OR STATIONARY OPERATIONS TO DISPLAY SIGN MESSAGES IT IS REFERRED TO AS AN ADVANCE WARNING VEHICLE. WHEN SIGNS ARE MOUNTED ON AN ADVANCED WARNING VEHICLE, THEY SHALL NOT OBSTRUCT VISIBILITY OF ANY LIGHTS (TAILLIGHTS OR WARNING LIGHTS), OR SIDE-VIEW MIRRORS ON THE VEHICLE, OR TRUCK MOUNTED ARROW BOARDS.
6. IN A MOVING OPERATION OR A STATIONARY OPERATION THAT OCCUPIES A LOCATION FOR UP TO 1 HOUR, THE OPERATOR SHALL REMAIN IN THE PROTECTIVE VEHICLE WITH THE SAFETY BELT AND HEADREST PROPERLY ADJUSTED, MAINTAIN VEHICLE SPACING, AND KEEP THE WHEELS ALIGNED WITH THE LANE STRIPING AND LANE TO MAINTAIN LANE DISCIPLINE AND TO STAY IN LANE IF STRUCK. THE PARKING BRAKE SHALL BE SET WHENEVER POSSIBLE. TWO-WAY RADIOS SHOULD BE USED TO COMMUNICATE BETWEEN THE OPERATOR AND THE WORK CREW.
7. IN A STATIONARY OPERATION THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR, ONCE THE PROTECTIVE VEHICLE HAS BEEN APPROPRIATELY PLACED, IT SHOULD BE UNOCCUPIED. UNOCCUPIED VEHICLE SHALL BE POSITIONED PARALLEL TO TRAFFIC, PARKING BRAKE SET, PLACED IN 2ND GEAR (MANUAL TRANSMISSIONS /ENGINE OFF) OR PARK / NEUTRAL (AUTOMATIC TRANSMISSIONS). THE FRONT WHEELS SHALL BE ALIGNED WITH THE LANE STRIPING AND LANE TO MAINTAIN LANE DISCIPLINE AND TO STAY IN LANE IF STRUCK.
8. NO WORK ACTIVITY, EQUIPMENT, VEHICLES AND/OR MATERIALS SHALL BE LOCATED BETWEEN THE PROTECTIVE VEHICLE AND THE ACTIVE WORK AREA (ROLL AHEAD DISTANCE).
9. DIRECT VERBAL COMMUNICATION BETWEEN THE PROTECTIVE VEHICLES AND THE WORK VEHICLE(S) / EQUIPMENT SHALL BE UTILIZED WHERE AVAILABLE.

WORK DURATION DEFINITIONS

- 1. THERE ARE MAINLY FIVE WORK DURATIONS:
A. LONG-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN 3 CONSECUTIVE DAYS.
B. INTERMEDIATE-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN ONE DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK LASTING MORE THAN 1 HOUR.
C. SHORT-TERM IS STATIONARY DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.
D. SHORT DURATION IS WORK THAT OCCUPIES A LOCATION UP TO 1 HOUR. IT CAN BE PERFORMED DURING THE DAYTIME OR AT NIGHT IN ACCORDANCE WITH NOTES N1 TO N11 NOTES ON NIGHTTIME WORK.
E. MOBILE IS WORK THAT MOVES INTERMITTENTLY OR CONTINUOUSLY WHERE THE WORK AT ANY SPECIFIC LOCATION COMPLETES WITHIN 15 MINUTES. IT IS USED FOR VEHICLE BASED OPERATIONS AND DOES NOT INVOLVE WORKERS ON FOOT. IT CAN BE PERFORMED DURING THE DAYTIME OR AT NIGHT IN ACCORDANCE WITH NOTES N1 TO N10 NOTES ON NIGHTTIME WORK.
2. SPECIAL OPERATIONS ARE WORK OPERATIONS THAT DO NOT FIT INTO ONE OF THE ABOVE FIVE CATEGORIES. SPECIAL OPERATIONS INCLUDE:
A. STOP AND GO OPERATIONS - WORK THAT COMPLETES WITHIN 5 MINUTES AND ALLOWS WORKERS ON FOOT.
B. OTHER OPERATIONS INCLUDING MOWING, MULCHING/HERBICIDE OPERATIONS, TEMPORARY ROAD/INTERSECTION CLOSURES, ETC.

ROADWAY TYPE DEFINITIONS

- 1. FREEWAY:
A. INTERSTATE: INTERREGIONAL HIGH-SPEED, HIGH-VOLUME, DIVIDED FACILITIES WITH COMPLETE CONTROL OF ACCESS.
B. PARKWAY: DIVIDED HIGHWAYS FOR NON-COMMERCIAL TRAFFIC WITH FULL CONTROL OF ACCESS, GRADE PARKWAY SEPARATIONS, INTERCHANGES, AND OCCASIONAL AT-GRADE INTERSECTIONS. PARKWAYS ARE DESIGNATED BY LAW.
2. EXPRESSWAY: DIVIDED HIGHWAYS FOR THROUGH TRAFFIC WITH FULL OR PARTIAL CONTROL OF ACCESS AND GENERALLY WITH GRADE SEPARATIONS AT MAJOR CROSSROADS. ALL FREEWAY STANDARD SHEETS ARE APPLICABLE TO EXPRESSWAY.
3. NON-FREEWAY:
A. MULTILANE DIVIDED HIGHWAY
B. MULTILANE UNDIVIDED HIGHWAY
C. TWO-LANE TWO-WAY ROADWAY

ALL NON-FREEWAYS CAN BE EITHER URBAN OR RURAL:

URBAN: (MEETS MORE THAN 1 OF THE FOLLOWING CRITERIA)

- HIGH DENSITY DEVELOPMENT
•ON-STREET PARKING
•VARIED BUILDING SETBACKS
•MULTI-STORY AND LOW-TO MEDIUM-RISE STRUCTURES FOR RESIDENTIAL, COMMERCIAL, AND EDUCATIONAL USES, STRUCTURES THAT ACCOMMODATE MIXED USES: COMMERCIAL, RESIDENTIAL, AND PARKING
•LIGHT INDUSTRIAL, AND SOMETIMES HEAVY INDUSTRIAL, LAND USE
•PROMINENT DESTINATIONS WITH SPECIALIZED STRUCTURES, E.G., LARGE THEATERS, SPORTS FACILITIES OR CONFERENCE CENTERS
•HIGH LEVELS OF PEDESTRIAN AND BICYCLIST ACTIVITY, WITH NEARLY CONTINUOUS SIDEWALKS AND MARKED CROSSWALKS
•HIGHER DENSITY OF TRANSIT STOPS AND ROUTES
•DRIVEWAY DENSITIES GREATER THAN 25 DRIVEWAYS/MILE ON EACH SIDE OF THE ROAD
•MINOR COMMERCIAL DRIVEWAY DENSITIES OF 10 DRIVEWAYS/MILE OR GREATER
•MAJOR COMMERCIAL DRIVEWAYS
•HIGH DENSITY OF CROSS STREETS

RURAL: DOES NOT MEET MORE THAN ONE OF THE ABOVE CRITERIA.

NOTES FOR NIGHTTIME OPERATIONS:

- N1. WORK OCCURRING AFTER SUNSET AND BEFORE SUNRISE WILL BE CONSIDERED NIGHTTIME OPERATIONS.
N2. ALL SIGNS, STOP/SLOW PADDLES AND RED FLAGS USED TO WARN/ALERT/CONTROL TRAFFIC SHALL BE RETROREFLECTIVE.
N3. ALL WORKERS INVOLVED SHALL WEAR PROTECTIVE HELMETS AND NIGHTTIME APPAREL IN ACCORDANCE WITH §107-05A. HIGH VISIBILITY APPAREL AT ALL TIMES.
N4. VEHICLES OPERATING ON THE PAVEMENT OF A CLOSED ROADWAY OR TRAVEL LANE SHALL DISPLAY ROTATING AMBER BEACONS OR FLASHING LED BEACONS AT ALL TIMES.
N5. LEVEL I ILLUMINATION SHALL BE PROVIDED NEAR THE BEGINNING OF LANE CLOSURE TAPERS AND AT ROAD CLOSURES, INCLUDING THE SETUP AND REMOVAL OF THE CLOSURE TAPERS.
N6. LEVEL II ILLUMINATION SHALL BE PROVIDED FOR FLAGGING STATIONS, ASPHALT PAVING, MILLING, AND CONCRETE PLACEMENT AND/OR REMOVAL OPERATIONS, INCLUDING BRIDGE DECKS, 50 FEET AHEAD OF AND 100 FEET BEHIND A PAVING OR MILLING MACHINE.
N7. LEVEL III ILLUMINATION SHALL BE PROVIDED FOR PAVEMENT OR STRUCTURAL CRACK FILLING, JOINT REPAIR, PAVEMENT PATCHING AND REPAIRS, INSTALLATION OF SIGNAL EQUIPMENT OR OTHER ELECTRICAL/MECHANICAL EQUIPMENT, AND OTHER TASKS INVOLVING FINE DETAILS OR INTRICATE PARTS AND EQUIPMENT.
N8. ALL LIGHTING SHALL BE DESIGNED, INSTALLED, AND OPERATED TO AVOID GLARE THAT AFFECTS TRAFFIC ON THE ROADWAY OR THAT CAUSES ANNOYANCE OR DISCOMFORT FOR RESIDENCES ADJOINING THE ROADWAY.
N9. PRIOR TO THE START OF NIGHTTIME OPERATIONS, A WRITTEN NIGHTTIME OPERATIONS AND LIGHTING PLAN IS REQUIRED FOR APPROVAL FROM THE DOT ENGINEER.
N10. SEE STANDARD SPECIFICATIONS §619 FOR ADDITIONAL REQUIREMENTS AND CONSIDERATIONS. REFER TO SECTION 619-3.19B FOR BALLOON LIGHTING REQUIREMENTS.
N11. FLAGGERS SHALL USE A FLASHLIGHT WITH RED GLOW CONE/RED LED BATON FOR FLAGGING IN NON-ILLUMINATED FLAGGER STATIONS DURING NIGHTTIME OPERATIONS.

NEW YORK STATE OF OPPORTUNITY Department of Transportation
U.S. CUSTOMARY STANDARD SHEET
WORK ZONE TRAFFIC CONTROL GENERAL NOTES
APPROVED DECEMBER 21, 2022 ISSUED UNDER EI 22-033
Robert Limoges ROBERT LIMOGES, P.E. DIRECTOR, OTSM
ERRATA 1 EFF. 05/01/2023 ISSUED WITH EB 22-033
619-010

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USER = rfoote

TABLE 011-01: PROTECTIVE VEHICLE REQUIREMENTS

CLOSURE TYPE	DURATION	MOBILE OPERATION AND STOP & GO				SHORT DURATION OPERATION				SHORT TERM OPERATION				INTERMEDIATE TERM OPERATION				LONG TERM OPERATION				
		ROAD TYPE & SPEED	FREEWAY	NON-FREEWAY			FREEWAY	NON-FREEWAY			FREEWAY	NON-FREEWAY			FREEWAY	NON-FREEWAY			FREEWAY	NON-FREEWAY		
				≥ 45 MPH	35 - 40 MPH	≤ 30 MPH		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH
LANE CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR WORK VEHICLE EXPOSED TO TRAFFIC	PVH+TMIA	PVH+TMIA	PVL+TMIA	PVL	PVH+TMIA	PVH+TMIA	PVL+TMIA	PVL	PVH+TMIA	PVH+TMIA	PVL+TMIA	PVL	PVH+TMIA	PVH+TMIA	PVL+TMIA	SEE NOTE 2	PVH+TMIA	PVH+TMIA	PVL+TMIA	SEE NOTE 2	
	-NO WORKERS ON FOOT -NO WORK VEHICLE EXPOSED TO TRAFFIC -OTHER HAZARDS EXPOSED (IE EQUIPMENT, MATERIALS)					PVH+TMIA	PVH+TMIA	PVL	PVL	PVH+TMIA	PVH+TMIA	PVL	SEE NOTE 2	PVH+TMIA	PVH+TMIA	SEE NOTE 2	SEE NOTE 2	PVH+TMIA	PVH+TMIA	SEE NOTE 2	SEE NOTE 2	
SHOULDER CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR WORK VEHICLE EXPOSED TO TRAFFIC	PVH+TMIA	PVH+TMIA	PVL+TMIA	PVL	PVH+TMIA	PVH+TMIA	PVL	PVL	PVH+TMIA	PVH+TMIA	PVL	PVL	PVH+TMIA	PVH+TMIA	SEE NOTE 2	SEE NOTE 2	PVH+TMIA	PVH+TMIA	SEE NOTE 2	SEE NOTE 2	
	-NO WORKERS ON FOOT -NO WORK VEHICLE EXPOSED TO TRAFFIC -OTHER HAZARDS EXPOSED (IE EQUIPMENT, MATERIALS, EXCAVATION)					PVH+TMIA	PVH+TMIA	PVL	PVL	PVH+TMIA	PVH+TMIA	PVL	SEE NOTE 2	PVH+TMIA	SEE NOTE 3	SEE NOTE 2	SEE NOTE 2	PVH+TMIA	SEE NOTE 3	SEE NOTE 2	SEE NOTE 2	

LEGEND
 PVL - PROTECTIVE VEHICLE LIGHT (MINIMUM GROSS WEIGHT 9,500 LBS. OR GREATER) (SEE NOTE 5)
 PVH - PROTECTIVE VEHICLE HEAVY (MINIMUM GROSS WEIGHT 22,000 LBS. OR GREATER)
 TMIA - TRUCK/TRAILER MOUNTED IMPACT ATTENUATOR

NOTES
 1. THE EXPOSURE CONDITIONS ASSUME THERE IS NO POSITIVE PROTECTION PRESENT.
 2. EITHER A PROTECTIVE LIGHT (PVL) OR THE STANDARD BUFFER SPACE (SEE TABLE 011-03) SHALL BE PROVIDED.
 3. EITHER A PROTECTIVE VEHICLE HEAVY (PVH) OR THE STANDARD BUFFER SPACE (SEE TABLE 011-03) SHALL BE PROVIDED.
 4. TRUCK/TRAILER MOUNTED IMPACT ATTENUATORS (TMIA) SHALL NOT BE MOUNTED/INSTALLED ON VEHICLES WITH A GROSS VEHICLE WEIGHT (GVW) LESS THAN WHAT IS MINIMALLY REQUIRED BY THE MANUFACTURER OF THE TMIA.
 5. THE USE OF A PROTECTIVE VEHICLE LIGHT (PVL) AS A SHADOW VEHICLE IS LIMITED TO NON-FREEWAY ROADWAYS WHERE THE POSTED SPEED LIMITS IS ≤ 40 MPH UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.

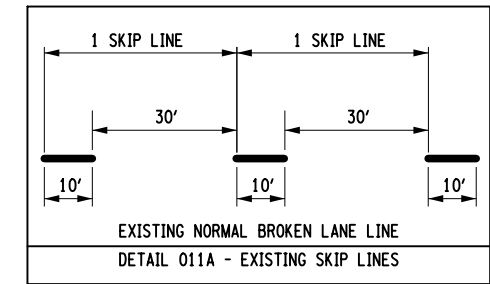


TABLE 011-02: TAPER LENGTHS & NUMBER OF CONES CHART

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	TAPER LENGTH: L (FT.) / * OF SKIP LINES / * OF CHANNELIZING DEVICES										SHOULDER TAPER LENGTH: L/3 (FT.) / * OF SKIP LINES / * OF CHANNELIZING DEVICES		
	LATERAL SHIFT OF TRAFFIC FLOW PATH (FT.)										FOR SHOULDER WIDTH		
	4	5	6	7	8	9	10	11	12	13	14	≤ 4 FT.	5 - 7 FT.
25	40/1/2	80/2/3	80/2/3	80/2/3	80/2/3	120/3/4	120/3/4	120/3/4	120/3/4	120/3/4	40/1/2	40/1/2	40/1/2
30	80/2/3	80/2/3	80/2/3	120/3/4	120/3/4	160/4/5	160/4/5	160/4/5	200/5/6	200/5/6	40/1/2	40/1/2	40/1/2
35	80/2/3	120/3/4	120/3/4	160/4/5	160/4/5	200/5/6	200/5/6	240/6/7	240/6/7	240/6/7	40/1/2	40/1/2	80/2/3
40	120/3/4	160/4/5	160/4/5	200/5/6	240/6/7	240/6/7	280/7/8	320/8/9	320/8/9	320/8/9	40/1/2	80/2/3	80/2/3
45	200/5/6	240/6/7	280/7/8	320/8/9	360/9/10	400/10/11	440/11/12	520/13/14	560/14/15	560/14/15	80/2/3	80/2/3	120/3/4
50	200/5/6	240/6/7	320/8/9	360/9/10	400/10/11	440/11/12	520/13/14	560/14/15	600/15/16	600/15/16	80/2/3	120/3/4	160/4/5
55	240/6/7	280/7/8	320/8/9	400/10/11	440/11/12	520/13/14	560/14/15	600/15/16	680/17/18	680/17/18	80/2/3	120/3/4	160/4/5
60	240/6/7	320/8/9	360/9/10	440/11/12	480/12/13	560/14/15	600/15/16	680/17/18	720/18/19	720/18/19	80/2/3	120/3/4	160/4/5
65	280/7/8	320/8/9	400/10/11	480/12/13	520/13/14	600/15/16	640/16/17	720/18/19	800/19/20	800/19/20	80/2/3	160/4/5	200/5/6

* THIS TABLE WAS CREATED WITH REFERENCE TO MUTCD TABLE 6H-4.
 ** THE NUMBER OF CHANNELIZING DEVICES SHOWN IS CALCULATED BASED ON A 40FT DEVICE SPACING. THE NUMBER OF CHANNELIZING DEVICES CAN BE ADJUSTED AS NECESSARY.

TABLE 011-03: LONGITUDINAL BUFFER SPACE

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	DISTANCE (FT.) / * OF SKIP LINES
25	155 / 4
30	200 / 5
35	250 / 6
40	305 / 8
45	360 / 9
50	425 / 11
55	495 / 13
65	645 / 16

* THIS TABLE IS THE SAME AS MUTCD TABLE 6C-2.

TABLE 011-04: ROLL AHEAD DISTANCE FOR PROTECTIVE VEHICLES

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	ROLL AHEAD DISTANCE (FT.) / * OF SKIP LINES FOR VEHICLES			
	PROTECTIVE VEHICLES WEIGHING 9,500 TO 21,999 LBS. GVW		PROTECTIVE VEHICLES WEIGHING 22,000 LBS. OR GREATER GVW	
	STATIONARY OPERATION	MOVING OPERATION (15 MPH MAX.)	STATIONARY OPERATION	MOVING OPERATION (15 MPH MAX.)
≥ 60	200/5	240/6	160/4	200/5
45 - 55	160/4	200/5	120/3	160/4
≤ 40	120/3	120/3	80/2	120/3

TABLE 011-05: FLARE RATES FOR POSITIVE BARRIER

TYPE OF POSITIVE BARRIER	POSTED SPEED LIMIT				
	30 MPH	40 MPH	50 MPH	55 MPH	65 MPH
TEMPORARY POSITIVE BARRIER	8:1	11:1	14:1	16:1	20:1
BOX BEAM OR HEAVY POST CORRUGATED BEAM	7:1	9:1	11:1	12:1	15:1

TABLE 011-06: ADVANCE WARNING SIGN SPACING

ROAD TYPE	DISTANCE BETWEEN SIGNS			SIGN LEGEND	
	A (FT.)	B (FT.)	C (FT.)	XX	YY
URBAN (≤ 30 MPH*)	100	100	100	AHEAD	AHEAD
URBAN (35-40 MPH*)	200	200	200	AHEAD	AHEAD
URBAN (≥ 45 MPH*)	350	350	350	1000 FT.	AHEAD
RURAL	500	500	500	1500 FT.	1000 FT.
FREEWAY	1000	1500	2640	1 MILE	1/2 MILE

* PRECONSTRUCTION POSTED SPEED LIMIT.
 ** THIS TABLE IS THE SAME AS MUTCD TABLE NY6H-3.

TABLE 011-07: TAPER LENGTH FOR TEMPORARY TRAFFIC CONTROL ZONES

TYPE OF TAPER	TAPER LENGTH (L)
MERGING TAPER	L
SHIFTING TAPER	L/2
SHOULDER TAPER	L/3
ONE-LANE, TWO-WAY TRAFFIC TAPER	50 FT. MIN -100 FT. MAX
DOWNSTREAM TAPER	50 FT. MIN -100 FT. MAX

* THIS TABLE IS THE SAME AS MUTCD TABLE 6C-3.

WORK ZONE TRAFFIC CONTROL LEGEND

SYMBOL	DESCRIPTION
	SIGN, TEMPORARY
	SPOTTER
	TEMPORARY POSITIVE BARRIER
	TEMPORARY POSITIVE BARRIER WITH WARNING LIGHTS
	TEMPORARY TRAFFIC SIGNAL HEAD
	TYPE III BARRICADE
	WARNING LIGHTS
	WORK AREA
	WORK VEHICLE
	WORK VEHICLE (MULCHING/HERBICIDE OPERATION)
	WORK VEHICLE (PAVEMENT MARKING)
	WORK VEHICLE (SIGNAL WORK)
	PROTECTIVE VEHICLE
	PROTECTIVE VEHICLE LIGHT
	PROTECTIVE VEHICLE HEAVY
	TRUCK/TRAILER MOUNTED IMPACT ATTENUATOR (TMIA)

WORK ZONE TRAFFIC CONTROL LEGEND

SYMBOL	DESCRIPTION
	ARROW PANEL
	ARROW PANEL, CAUTION MODE
	ARROW PANEL TRAILER OR SUPPORT
	CHANGEABLE MESSAGE SIGN (PVMS)
	CHANNELIZING DEVICE
	CONE
	CRASH CUSHION/TEMPORARY IMPACT ATTENUATOR
	DIRECTION OF TEMPORARY TRAFFIC DETOUR
	DIRECTION OF TRAFFIC
	AUTOMATED FLAGGER ASSISTANCE DEVICE WITH OPERATOR
	FLAGGER
	FLAG TREE
	LUMINAIRE
	MOWER
	PARKWAY GRASS SHOULDER
	PAVEMENT MARKINGS THAT SHALL BE REMOVED FOR A LONG TERM PROJECT
	PORTABLE VARIABLE MESSAGE SIGN
	ADVANCE WARNING SIGN WITH ORANGE FLAGS
	TRAILER FOR ARROW PANEL OR PORTABLE VARIABLE MESSAGE SIGN (PVMS)

NEW YORK STATE OF OPPORTUNITY Department of Transportation

U.S. CUSTOMARY STANDARD SHEET

WORK ZONE TRAFFIC CONTROL GENERAL TABLES AND LEGEND

APPROVED DECEMBER 21, 2022 ISSUED UNDER EI 22-033

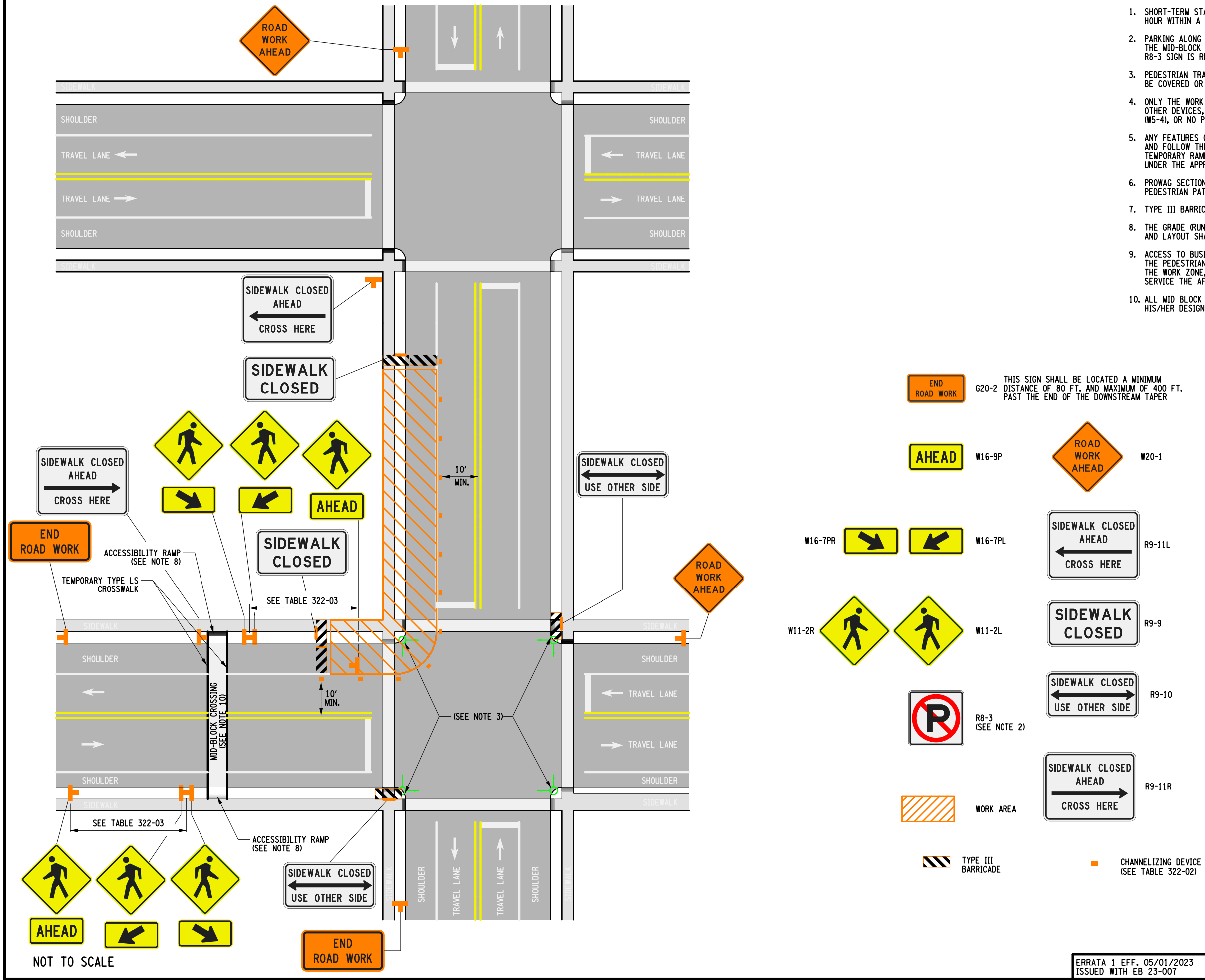
Robert Limoges ROBERT LIMOGES, P.E. DIRECTOR, OTSM

619-011

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NOTES:

1. SHORT-TERM STATIONARY IS DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.
2. PARKING ALONG THE CURB SHALL BE PROHIBITED FOR AT LEAST 50' IN ADVANCE OF THE MID-BLOCK CROSSWALK. IF THE SHOULDER IS GREATER THAN OR EQUAL TO 8 FOOT, AN R8-3 SIGN IS RECOMMENDED TO PREVENT PARKING ON SHOULDER.
3. PEDESTRIAN TRAFFIC SIGNAL DISPLAYS CONTROLLING CLOSED CROSSWALKS SHOULD BE COVERED OR DEACTIVATED.
4. ONLY THE WORK ZONE TRAFFIC CONTROL DEVICES RELATED TO PEDESTRIANS ARE SHOWN. OTHER DEVICES, SUCH AS LANE/SHOULDER CLOSURE SIGNING, ROAD NARROWS SIGNS (W5-4), OR NO PARKING SIGNS MAY BE USED TO CONTROL VEHICULAR TRAFFIC.
5. ANY FEATURES CONSTRUCTED AS PART OF A PEDESTRIAN DETOUR MUST BE ADA COMPLIANT AND FOLLOW THE 2011 PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG). TEMPORARY RAMPS AND OTHER TEMPORARY PEDESTRIAN FACILITIES SHALL BE PAID FOR UNDER THE APPROPRIATE ITEM FOR ASPHALT SIDEWALKS.
6. PROWAG SECTION R205 AND MUTCD PART 6 SHALL APPLY TO ALL TEMPORARY ALTERNATIVE PEDESTRIAN PATHWAYS.
7. TYPE III BARRICADES SHALL BE THE FULL WIDTH OF THE PATH BEING CLOSED.
8. THE GRADE (RUNNING SLOPE), AND CROSS SLOPE OF A TEMPORARY CURB RAMP FOR DESIGN AND LAYOUT SHALL BE IN ACCORDANCE WITH STANDARD SHEET 608-01.
9. ACCESS TO BUSINESS ENTRANCES AND TRANSIT STOPS WILL NEED TO BE MAINTAINED. IF THE PEDESTRIAN FACILITY CURRENTLY HAS A TRANSIT STOP THAT WILL BE AFFECTED BY THE WORK ZONE, PRACTITIONERS SHOULD CONSULT THE TRANSIT AUTHORITIES THAT SERVICE THE AFFECTED STOP TO DETERMINE HOW TO ACCOMMODATE THE STOP.
10. ALL MID BLOCK CROSSWALKS SHALL BE APPROVED BY THE DOT REGIONAL DIRECTOR OR HIS/HER DESIGNEE.



END ROAD WORK G20-2 THIS SIGN SHALL BE LOCATED A MINIMUM DISTANCE OF 80 FT. AND MAXIMUM OF 400 FT. PAST THE END OF THE DOWNSTREAM TAPER

AHEAD W16-9P **ROAD WORK AHEAD** W20-1

W16-7PR **W16-7PL** **SIDEWALK CLOSED AHEAD CROSS HERE** R9-11L

W11-2R **W11-2L** **SIDEWALK CLOSED** R9-9

R8-3 (SEE NOTE 2) **SIDEWALK CLOSED USE OTHER SIDE** R9-10

SIDEWALK CLOSED AHEAD CROSS HERE R9-11R

TYPE III BARRICADE **CHANNELIZING DEVICE** (SEE TABLE 322-02)

REFER TO SHEET 2 OF 2 FOR ALL TABLES



U.S. CUSTOMARY STANDARD SHEET
WORK ZONE TRAFFIC CONTROL
TWO-LANE TWO-WAY ROADWAY
CROSSWALK CLOSURE AND PEDESTRIAN DETOUR
SHORT TERM OPERATIONS
 (SHEET 1 OF 2)

APPROVED APRIL 8, 2022 ISSUED UNDER EI 22-008
 Robert Limoges ROBERT LIMOGES, P.E. DIRECTOR, OTSM
 619-322

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NOT TO SCALE

SIGN	NON-FREEWAY	FREEWAY
G20-2	36x18	48x24
R8-3	24x24	48x48
R9-9**	24x12	24x12
R9-10**	24x12	24x12
R9-11L/R9-11R**	24x18	24x18
R11-2	48x30	48x30
W16-9P**	24x12	-
W16-7PR**	24x12	-
W11-2R/W11-2L	36x36	-
W20-1	36x36	48x48


*FREEWAY SIZES MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST.
**SIGNS NOT FOR FREEWAY USE.

WORK ZONE PROVISIONS	MAXIMUM DEVICE SPACING (CENTER TO CENTER)	MUTCD COMPLIANT CHANNELIZING DEVICE								
		DRUMS	STANDARD CONES	TALL CONES	EXTRA TALL CONES	TEMPORARY TUBULAR MARKERS	INTERIM TUBULAR MARKERS	VERTICAL PANELS	OVERSIZED VERTICAL PANELS	TYPE III BARRICADES
SHORT-TERM STATIONARY WORK ZONES INVOLVE WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.	20 FT.	X	X	X	X	-	-	X	X	-

NOTES: X= ALLOWED, BLANK = NOT ALLOWED

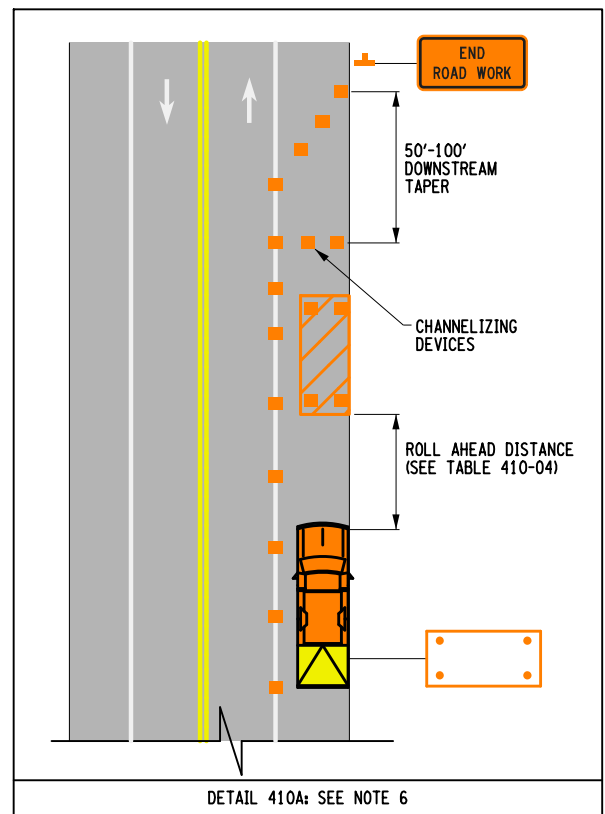
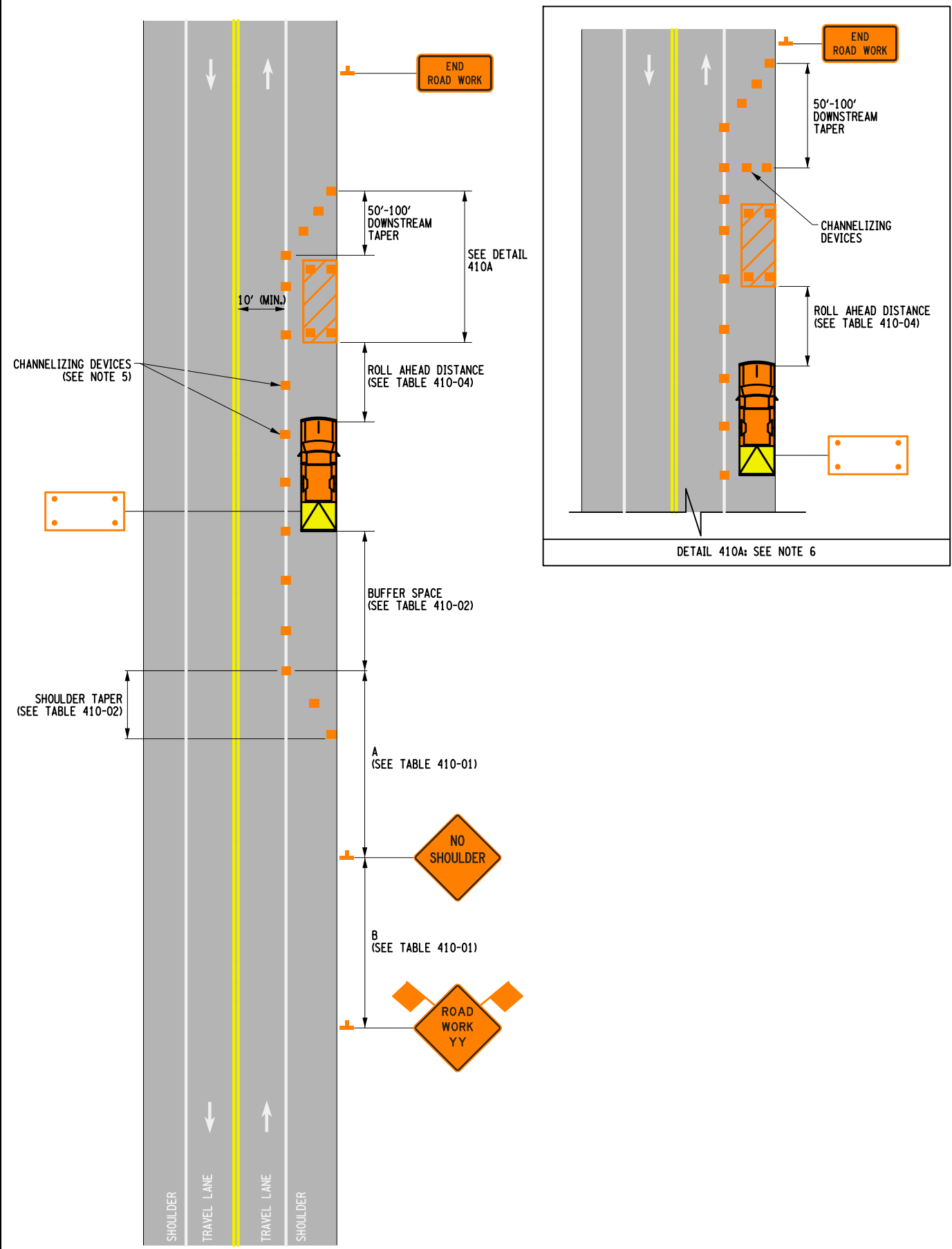
POSTED OR 85 TH PERCENTILE SPEED (MPH)	ADVANCE PLACEMENT DISTANCE (FT.) ¹												
	CONDITION A: SPEED REDUCTION AND LANE CHANGING IN HEAVY TRAFFIC ²	CONDITION B: DECELERATION TO THE LISTED ADVISORY SPEED (MPH) FOR THE CONDITION ⁴											
		0 ³	5	10	15	20	25	30	35	40	45	50	55
20	410	115	110	105	90	75	-	-	-	-	-	-	-
25	515	155	160	150	135	120	95	-	-	-	-	-	-
30	620	200	205	195	185	165	140	110	-	-	-	-	-
35	720	250	255	245	235	215	190	160	130	-	-	-	-
40	825	305	320	310	295	280	255	225	190	150	-	-	-
45	930	360	380	370	360	340	315	285	255	210	165	-	-
50	1030	425	455	450	435	415	390	360	330	285	240	185	-
55	1135	495	530	520	505	490	460	435	400	355	315	255	205

- NOTES:
- THE DISTANCES HAVE NOT BEEN MODIFIED TO ACCOUNT FOR SIGN LEGIBILITY.
 - TYPICAL CONDITIONS ARE LOCATIONS WHERE THE ROAD USER MUST USE EXTRA TIME TO ADJUST SPEED AND CHANGE LANES IN HEAVY TRAFFIC BECAUSE OF A COMPLEX DRIVING SITUATION. TYPICAL SIGNS ARE MERGE AND RIGHT LAND ENDS. THE DISTANCES ARE TAKEN FROM THE 2004 AASHTO POLICY, EXHIBIT 3-3, DECISION SIGHT DISTANCE, AVOIDANCE MANEUVER E.
 - TYPICAL CONDITION IS THE WARNING OF A POTENTIAL STOP SITUATION. TYPICAL SIGNS ARE STOP AHEAD, YIELD AHEAD, SIGNAL AHEAD, AND INTERSECTION WARNING SIGNS.
 - TYPICAL CONDITIONS ARE LOCATIONS WHERE THE ROAD USER MUST DECREASE SPEED TO MANEUVER THROUGH THE WARNED CONDITION. TYPICAL SIGNS ARE TURN, CURVE, REVERSE TURN, OR REVERSE CURVE. THE DISTANCES ARE DETERMINED BY PROVIDING A 2.5 SECOND PRT AND A VEHICLE DECELERATION RATE OF 10 FT./SECOND.²
 - TYPICAL SIGNS ARE ADDED LANE, ROAD NARROWS, DIVIDED HIGHWAY, AND CONDITION B SIGNS DISPLAYED WITHOUT AN ADVISORY SPEED.

 NEW YORK STATE OF OPPORTUNITY.	Department of Transportation
U.S. CUSTOMARY STANDARD SHEET	
WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY ROADWAY CROSSWALK CLOSURE AND PEDESTRIAN DETOUR SHORT TERM OPERATIONS (SHEET 2 OF 2)	
APPROVED APRIL 8, 2022	ISSUED UNDER EI 22-008
<i>Robert Limoges</i> ROBERT LIMOGES, P.E. DIRECTOR, OTSM	619-322

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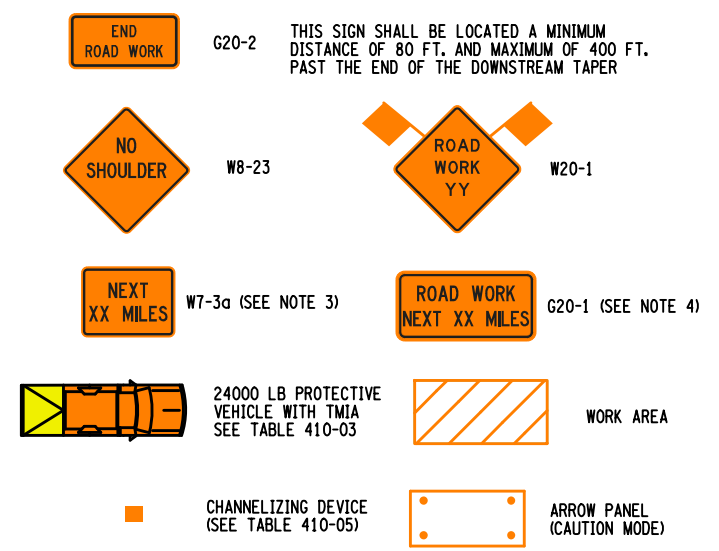


NOTES:

1. INTERMEDIATE-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN ONE DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK LASTING MORE THAN 1 HOUR.
2. NO WORK ACTIVITY OR STORAGE OF EQUIPMENT, VEHICLES, OR MATERIAL SHOULD OCCUR WITHIN A BUFFER SPACE.
3. XX IS THE EXPECTED OVERALL LENGTH OF THE OPERATION TO BE COMPLETED WITHIN THE WORK DAY. A SUPPLEMENTAL DISTANCE PLAQUE W7-3a SHALL BE USED WITH SIGN W20-1 WHEN THE DISTANCE BETWEEN THE ADVANCE WARNING SIGNS AND WORK MAY BECOME GREATER THAN 2 MILES AS A RESULT OF THE FOLLOWING SITUATIONS:
 - MULTIPLE WORK LOCATIONS ARE ANTICIPATED WITHIN XX MILES FROM THE W20-1 SIGN
 - WORK AREA WILL BE RELOCATED DURING THE DURATION OF THE WORK WITHIN XX MILES FROM THE W20-1 SIGN
 THE SUPPLEMENT SIGN W7-3a SHALL INDICATE THE MAXIMUM ANTICIPATED DISTANCE BETWEEN THE W20-1 SIGN AND THE FARTHEST WORK LOCATION.
4. WHEN MULTIPLE WORK LOCATIONS EXIST WITHIN XX MILES FROM THE W20-1 SIGN, A G20-1 SIGN SHALL BE PLACED EVERY TWO MILES INDICATING THE DISTANCE FROM THE SIGN TO THE FARTHEST WORK LOCATION.
5. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 20' IN THE ACTIVE WORK SPACE.
6. CHANNELIZING DEVICES SHALL BE PLACED TRANSVERSELY A MINIMUM OF EVERY 800' AS SHOWN WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 800'.
7. THE PROTECTIVE VEHICLE(S) SHALL MAINTAIN THE APPROPRIATE ROLL AHEAD DISTANCE, BE AN UNOCCUPIED TRUCK POSITIONED PARALLEL TO TRAFFIC, PARKING BRAKE SET, PLACED IN 2ND GEAR (MANUAL TRANSMISSIONS /ENGINE OFF) OR PARK / NEUTRAL (AUTOMATIC TRANSMISSIONS) AND HAVE THE FRONT WHEELS ALIGNED WITH THE LANE STRIPING.

NOTES ON NIGHTTIME WORK:

- N1. WORK OCCURRING AFTER SUNSET AND BEFORE SUNRISE WILL BE CONSIDERED NIGHTTIME OPERATIONS.
- N2. ALL SIGNS, STOP/SLOW PADDLES AND RED FLAGS USED TO WARN/ALERT/CONTROL TRAFFIC SHALL BE RETROREFLECTIVE.
- N3. ALL WORKERS INVOLVED SHALL WEAR PROTECTIVE HELMETS AND NIGHTTIME APPAREL IN ACCORDANCE WITH §107-05A. HIGH VISIBILITY APPAREL AT ALL TIMES.
- N4. VEHICLES OPERATING ON THE PAVEMENT OF A CLOSED ROADWAY OR TRAVEL LANE SHALL DISPLAY ROTATING AMBER BEACONS OR FLASHING LED BEACONS AT ALL TIMES.
- N5. LEVEL I ILLUMINATION SHALL BE PROVIDED NEAR THE BEGINNING OF LANE CLOSURE TAPERS AND AT ROAD CLOSURES, INCLUDING THE SETUP AND REMOVAL OF THE CLOSURE TAPERS.
- N6. LEVEL II ILLUMINATION SHALL BE PROVIDED FOR FLAGGING STATIONS, ASPHALT PAVING, MILLING, AND CONCRETE PLACEMENT AND/OR REMOVAL OPERATIONS, INCLUDING BRIDGE DECKS, 50 FEET AHEAD OF AND 100 FEET BEHIND A PAVING OR MILLING MACHINE.
- N7. LEVEL III ILLUMINATION SHALL BE PROVIDED FOR PAVEMENT OR STRUCTURAL CRACK FILLING, JOINT REPAIR, PAVEMENT PATCHING AND REPAIRS, INSTALLATION OF SIGNAL EQUIPMENT OR OTHER ELECTRICAL/MECHANICAL EQUIPMENT, AND OTHER TASKS INVOLVING FINE DETAILS OR INTRICATE PARTS AND EQUIPMENT.
- N8. ALL LIGHTING SHALL BE DESIGNED, INSTALLED, AND OPERATED TO AVOID GLARE THAT AFFECTS TRAFFIC ON THE ROADWAY OR THAT CAUSES ANNOYANCE OR DISCOMFORT FOR RESIDENCES ADJOINING THE ROADWAY.
- N9. PRIOR TO THE START OF NIGHTTIME OPERATIONS, A WRITTEN NIGHTTIME OPERATIONS AND LIGHTING PLAN IS REQUIRED FOR APPROVAL.
- N10. SEE NIGHTTIME SAFETY BULLETIN, HDM §16.5.7, & STANDARD SPECIFICATIONS §619 FOR ADDITIONAL REQUIREMENTS AND CONSIDERATIONS.



NOT TO SCALE

REFER TO SHEET 2 OF 2 FOR ALL TABLES

NEW YORK STATE OF OPPORTUNITY.	Department of Transportation
U.S. CUSTOMARY STANDARD SHEET	
WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY ROADWAY SHOULDER CLOSURE INTERMEDIATE TERM OPERATION (SHEET 1 OF 2)	
APPROVED DECEMBER 2, 2021	ISSUED UNDER EI 21-028
<i>Robert Limoges</i> ROBERT LIMOGES, P.E. DIRECTOR, OTSM	619-410

TABLE 410-01: ADVANCE WARNING SIGN SPACING		
ROAD TYPE	DISTANCE BETWEEN SIGNS	
	A (FT.)	B (FT.)
URBAN (≤ 30 MPH*)	100	100
URBAN (35-40 MPH*)	200	200
URBAN (≥ 45 MPH*)	350	350
RURAL	500	500

* PRECONSTRUCTION POSTED SPEED LIMIT

TABLE 410-02: LONGITUDINAL BUFFER SPACE AND TAPER LENGTHS							
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	LONGITUDINAL BUFFER SPACE DISTANCE (FT.)/* OF SKIP LINES	TAPER LENGTH: L (FT.)/* OF SKIP LINES/* OF CHANNELIZING DEVICES			SHOULDER TAPER LENGTH: L/3 (FT.)/* OF SKIP LINES/* OF CHANNELIZING DEVICES		
		FOR LANE WIDTH IN FT. (LATERAL SHIFT OF TRAFFIC FLOW PATH)			FOR SHOULDER WIDTH		
		10	11	12	≤ 4 FT.	5 - 7 FT.	≥ 8 FT.
25	155/4	120/3/4	120/3/4	120/3/4	40/1/2	40/1/2	40/1/2
30	200/5	160/4/5	160/4/5	200/5/6	40/1/2	40/1/2	40/1/2
35	250/6	200/5/6	240/6/7	240/6/7	40/1/2	40/1/2	80/2/3
40	305/8	280/7/8	320/8/9	320/8/9	40/1/2	80/2/3	80/2/3
45	360/9	440/11/12	520/13/14	560/14/15	80/2/3	80/2/3	120/3/4
50	425/11	520/13/14	560/14/15	600/15/16	80/2/3	120/3/4	160/4/5
55	495/13	560/14/15	600/15/16	680/17/18	80/2/3	120/3/4	160/4/5

TABLE 410-03: PROTECTIVE VEHICLE REQUIREMENTS				
CLOSURE TYPE	ROAD TYPE & SPEED	NON-FREEWAY		
		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH
LANE CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	P, TMIA	SEE NOTE 2
	OTHER HAZARDS NO WORKERS EXPOSED	P, TMIA	SEE NOTE 2	SEE NOTE 2
SHOULDER CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	SEE NOTE 2	SEE NOTE 2
	OTHER HAZARDS NO WORKERS EXPOSED	SEE NOTE 2	SEE NOTE 2	SEE NOTE 2

LEGEND
P: PROTECTIVE VEHICLE REQUIRED FOR EACH CLOSED LANE & EACH CLOSED PAVED SHOULDER 8' OR WIDER, IF THE WORK SPACE MOVES WITHIN THE STATIONARY CLOSURE, THE PROTECTIVE VEHICLE SHALL BE REPOSITIONED ACCORDINGLY
TMIA: TMIA REQUIRED
NOTES:
1. THE EXPOSURE CONDITIONS ASSUME THERE IS NO POSITIVE PROTECTION PRESENT
2. EITHER A PROTECTIVE VEHICLE OR THE STANDARD BUFFER SPACE SHALL BE PROVIDED


TABLE 410-04: ROLL AHEAD DISTANCE		
ROLL AHEAD DISTANCE (FT.)/* OF SKIP LINES FOR VEHICLES		
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	STATIONARY OPERATION	
	MIN	MAX
≥ 55	120/3	200/5
45 - 50	80/2	160/4
≤ 40	40/1	120/3

TABLE 410-05: CHANNELIZING DEVICE APPLICATION FOR INTERMEDIATE-TERM STATIONARY WORK ZONES										
WORK ZONE PROVISIONS	MAXIMUM DEVICE SPACING (CENTER TO CENTER)	MUTCD COMPLIANT CHANNELIZING DEVICE								
		DRUMS	STANDARD CONES	TALL CONES	EXTRA TALL CONES	TEMPORARY TUBULAR MARKERS	INTERIM TUBULAR MARKERS	VERTICAL PANELS	OVERSIZED VERTICAL PANELS	TYPE III BARRICADES
INTERMEDIATE-TERM STATIONARY WORK ZONES INVOLVE WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR	20 FT. *	X							X	
	40 FT.	X							X	
MARKING FOR TRANSVERSE BUMPS ¹	N/A	X ²	X		X ²				X ²	
TRANSVERSE DEVICE WITHIN CLOSED TRAFFIC LANE AND/OR SHOULDER	800 FT.	X		X	X			X	X	0
REMOVAL OF EXISTING GUIDE RAIL	80 FT.	X		X	X	X		X	X	0
	40 FT.	X		X	X	X		X	X	0

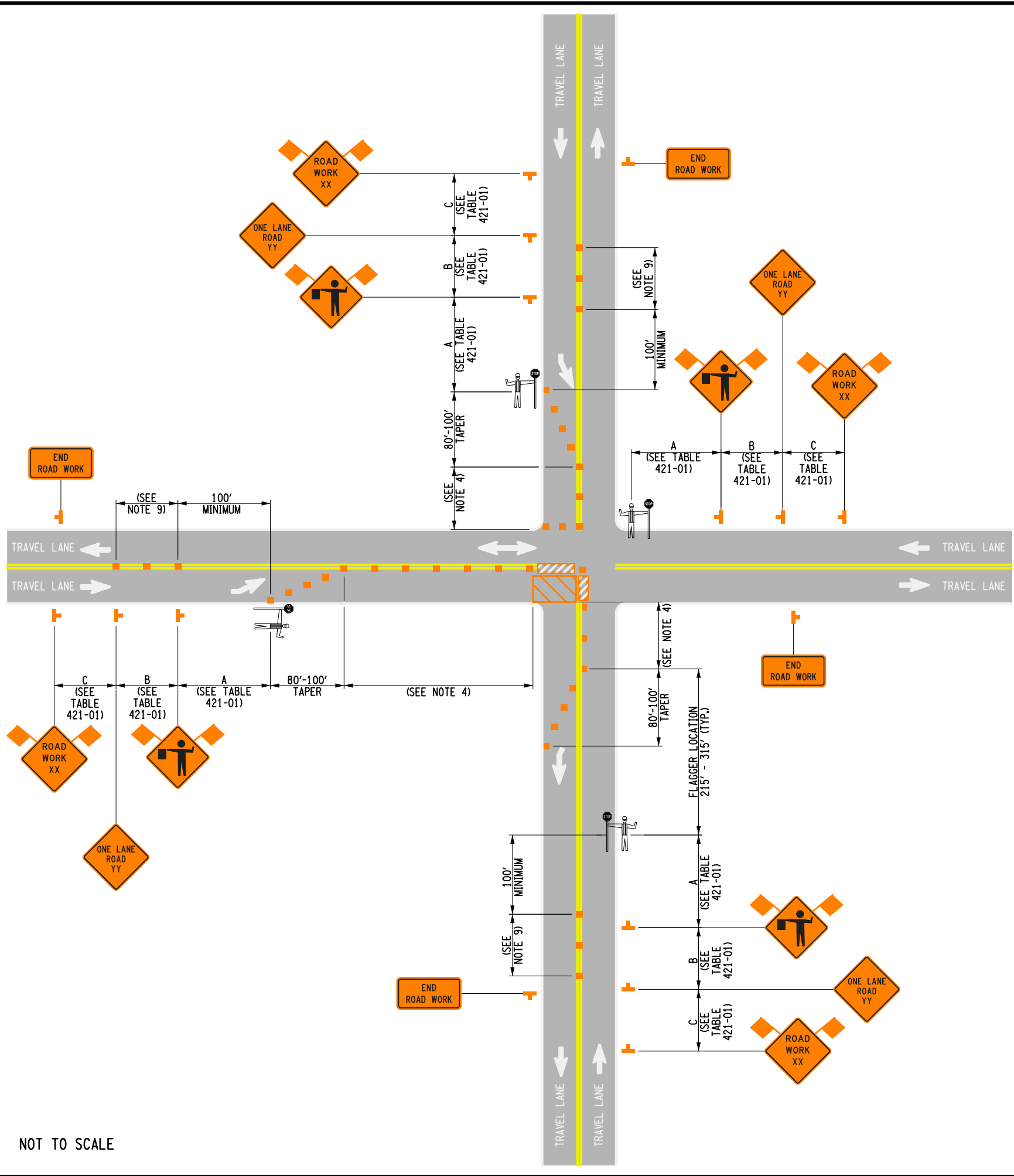
NOTES: X= ALLOWED, BLANK = NOT ALLOWED, 0 = OPTIONAL
1. - A TYPE 1 OBJECT MARKER MAY BE USED IN LIEU OF CHANNELIZING DEVICE.
2. - CHANNELIZING DEVICES SHALL BE EQUIPPED WITH A FLASHING WARNING LIGHT.
* SEE NOTE 5 ON SHEET 1 OF 2.

TABLE 410-06: REQUIRED SIGN SIZES*		
SIGN	NON-FREEWAY	FREEWAY
G20-2	36x18	48x24
W20-1	36x36	48x48
W21-5	36x36	48x48
W7-3a	24x18	36x30
G20-1	36x18	48x24
WARNING FLAG	18x18	18x18

*FREEWAY SIZES MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST.

 NEW YORK STATE OF OPPORTUNITY.	Department of Transportation
U.S. CUSTOMARY STANDARD SHEET	
WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY ROADWAY SHOULDER CLOSURE INTERMEDIATE TERM OPERATION (SHEET 2 OF 2)	
APPROVED APRIL 8, 2022 <i>Robert Limoges</i> ROBERT LIMOGES, P.E. DIRECTOR, OTSM	ISSUED UNDER EI 22-008 619-410

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NYR9-11 (SEE NOTE 11)

W3-4 (SEE NOTE 5)

W20-7a (SEE NOTE 6)

W20-1

W20-4 (SEE NOTE 6)

G20-2 THIS SIGN SHALL BE LOCATED A MINIMUM DISTANCE OF 80 FT. AND MAXIMUM OF 400 FT. PAST THE END OF THE DOWNSTREAM TAPER

WORK AREA

FLAGGER

CHANNELIZING DEVICE (SEE TABLE 421-02)

TYPE III BARRICADE

NOTES:

- INTERMEDIATE-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN ONE DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK LASTING MORE THAN 1 HOUR.
 - AT SIGNALIZED INTERSECTIONS, SIGNALS SHALL BE TURNED OFF FOR ANY FLAGGING OPERATIONS, UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.
 - CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 20' IN THE ACTIVE WORK SPACE.
 - IF CONDITIONS WARRANT, PROTECTIVE VEHICLE WITH APPROPRIATE ROLL AHEAD DISTANCE MAY BE USED IN ADVANCE OF THE WORK AREA. TO USE PROTECTIVE VEHICLE, BUFFER SPACE SHALL BE PROVIDED ACCORDINGLY AND THE VEHICLE SHALL BE POSITIONED PARALLEL TO TRAFFIC AND HAVE THE WHEELS ALIGNED WITH THE LANE STRIPING.
 - IF THE TRAFFIC IS EXPECTED TO QUEUE PAST THE W20-4 SIGN, A W3-4 SIGN SHOULD BE ADDED HALFWAY BETWEEN THE W20-4 AND W20-1 SIGNS.
 - FLAGGER SIGN (W20-7a) AND ONE LANE ROAD AHEAD SIGN (W20-4) SHALL BE REMOVED, COVERED OR TURNED AWAY FROM ROAD USERS WHEN FLAGGING OPERATIONS ARE NOT OCCURRING.
 - ALL FLAGGERS SHALL USE 24" (MIN.) OCTAGON SHAPED STOP/SLOW PADDLES HAVING 6" STAFF. THE PADDLE IS THE PREFERRED DEVICE, BUT THE FLAG MAY BE USED AT INTERSECTIONS WHERE THE STOP/SLOW PADDLE WOULD OFFER CONTRADICTING INFORMATION TO DRIVERS TRAVELING IN OPPOSITE DIRECTIONS/LEGS OF THE INTERSECTION OR DURING INCIDENT MANAGEMENT SITUATIONS.
 - ADDITIONAL FLAGGERS SHALL BE LOCATED AT ALL INTERSECTIONS AND COMMERCIAL DRIVEWAYS LOCATED WITHIN OR NEAR THE ACTIVE WORK AREA.
 - CENTERLINE CONES MAY BE ADDED TO ENHANCE THE VISIBILITY OF THE FLAGGER STATION. IF CONES ARE USED, PLACE THEM 100 FT. (MINIMUM) FROM FLAGGER.
 - NO WORK ACTIVITY, EQUIPMENT, OR STORAGE OF VEHICLES, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME.
 - THE NYR9-11 SIGN IS RECOMMENDED. WHEN USED, IT SHALL BE PLACED IN ADVANCE OF THE FIRST ADVANCE WARNING SIGN. THE PLACEMENT DISTANCE SHALL BE 1000' FOR POSTED SPEED LIMITS OF 45 MPH OR HIGHER, AND 300' - 500' FOR POSTED SPEED LIMITS OF LESS THAN 45 MPH.
- NOTES ON NIGHTTIME WORK:
- WORK OCCURRING AFTER SUNSET AND BEFORE SUNRISE WILL BE CONSIDERED NIGHTTIME OPERATIONS.
 - ALL SIGNS, STOP/SLOW PADDLES AND RED FLAGS USED TO WARN/ALERT/CONTROL TRAFFIC SHALL BE RETROREFLECTIVE.
 - ALL WORKERS INVOLVED SHALL WEAR PROTECTIVE HELMET AND NIGHTTIME APPAREL IN ACCORDANCE WITH §107-05A. HIGH VISIBILITY APPAREL AT ALL TIMES.
 - VEHICLES OPERATING ON THE PAVEMENT OF A CLOSED ROADWAY OR TRAVEL LANE SHALL DISPLAY ROTATING AMBER BEACONS OR FLASHING LED BEACON AT ALL TIMES.
 - LEVEL I ILLUMINATION SHALL BE PROVIDED NEAR THE BEGINNING OF LANE CLOSURE TAPERS AND AT ROAD CLOSURES, INCLUDING THE SETUP AND REMOVAL OF THE CLOSURE TAPERS.
 - LEVEL II ILLUMINATION SHALL BE PROVIDED FOR FLAGGING STATIONS, ASPHALT PAVING, MILLING, AND CONCRETE PLACEMENT AND/OR REMOVAL OPERATIONS, INCLUDING BRIDGE DECKS, 50 FEET AHEAD OF AND 100 FEET BEHIND A PAVING OR MILLING MACHINE.
 - LEVEL III ILLUMINATION SHALL BE PROVIDED FOR PAVEMENT OR STRUCTURAL CRACK FILLING, JOINT REPAIR, PAVEMENT PATCHING AND REPAIRS, INSTALLATION OF SIGNAL EQUIPMENT OR OTHER ELECTRICAL/MECHANICAL, AND OTHER TASKS INVOLVING FINE DETAILS OR INTRICATE PARTS AND EQUIPMENT.
 - ALL LIGHTING SHALL BE DESIGNED, INSTALLED, AND OPERATED TO AVOID GLARE THAT AFFECTS TRAFFIC ON THE ROADWAY OR THAT CAUSES ANNOYANCE OR DISCOMFORT FOR RESIDENCES ADJOINING THE ROADWAY.
 - PRIOR TO THE START OF NIGHTTIME OPERATIONS, A WRITTEN NIGHTTIME OPERATIONS AND LIGHTING PLAN IS REQUIRED FOR APPROVAL.
 - SEE NIGHTTIME SAFETY BULLETIN, HDM §16.5.7, & STANDARD SPECIFICATIONS §619 FOR ADDITIONAL REQUIREMENTS AND CONSIDERATIONS.

REFER TO SHEET 2 OF 2 FOR ALL TABLES

NEW YORK STATE OF OPPORTUNITY.		Department of Transportation	
U.S. CUSTOMARY STANDARD SHEET			
WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY ROADWAY FLAGGING OPERATION AT INTERSECTION INTERMEDIATE TERM OPERATION (SHEET 1 OF 2)			
APPROVED DECEMBER 2, 2021		ISSUED UNDER EI 21-028	
Robert Limoges ROBERT LIMOGES, P.E. DIRECTOR, OTSM		619-421	

NOT TO SCALE

TABLE 421-01: ADVANCE WARNING SIGN SPACING					
ROAD TYPE	DISTANCE BETWEEN SIGNS			SIGN LEGEND	
	A (FT.)	B (FT.)	C (FT.)	XX	YY
URBAN (≤ 30 MPH*)	100	100	100	AHEAD	AHEAD
URBAN (35-40 MPH*)	200	200	200	AHEAD	AHEAD
URBAN (≥ 45 MPH*)	350	350	350	1000 FT.	AHEAD
RURAL	500	500	500	1500 FT.	1000 FT.

* PRECONSTRUCTION POSTED SPEED LIMIT

TABLE 421-02: CHANNELIZING DEVICE APPLICATION FOR INTERMEDIATE-TERM STATIONARY WORK ZONES										
WORK ZONE PROVISIONS	MAXIMUM DEVICE SPACING (CENTER TO CENTER)	MUTCD COMPLIANT CHANNELIZING DEVICE								
		DRUMS	STANDARD CONES	TALL CONES	EXTRA TALL CONES	TEMPORARY TUBULAR MARKERS	INTERIM TUBULAR MARKERS	VERTICAL PANELS	OVERSIZED VERTICAL PANELS	TYPE III BARRICADES
INTERMEDIATE-TERM STATIONARY WORK ZONES INVOLVE WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR	20 FT. *	X							X	
	40 FT.	X							X	
MARKING FOR TRANSVERSE BUMPS ¹	N/A	X ²			X ²				X ²	
TRANSVERSE DEVICE WITHIN CLOSED TRAFFIC LANE AND/OR SHOULDER	800 FT.	X		X	X			X	X	0
REMOVAL OF EXISTING GUIDE RAIL	80 FT.	X		X	X	X		X	X	0
	40 FT.									

NOTES: X= ALLOWED, BLANK = NOT ALLOWED, 0 = OPTIONAL
 1. - A TYPE 1 OBJECT MARKER MAY BE USED IN LIEU OF CHANNELIZING DEVICE.
 2. - CHANNELIZING DEVICES SHALL BE EQUIPPED WITH A FLASHING WARNING LIGHT.

* SEE NOTE 3 ON SHEET 1 OF 2.

TABLE 421-03: REQUIRED SIGN SIZES*		
SIGN	NON-FREEWAY	FREEWAY
W20-1	36x36	48x48
W20-4	36x36	48x48
W20-7A	36x36	48x48
G20-2	36x18	48x24
WARNING FLAG	18x18	18x18

*FREEWAY SIZES MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST.



Department of Transportation

U.S. CUSTOMARY STANDARD SHEET

WORK ZONE TRAFFIC CONTROL
 TWO-LANE TWO-WAY ROADWAY
 FLAGGING OPERATION AT INTERSECTION
 INTERMEDIATE TERM OPERATION
 (SHEET 2 OF 2)

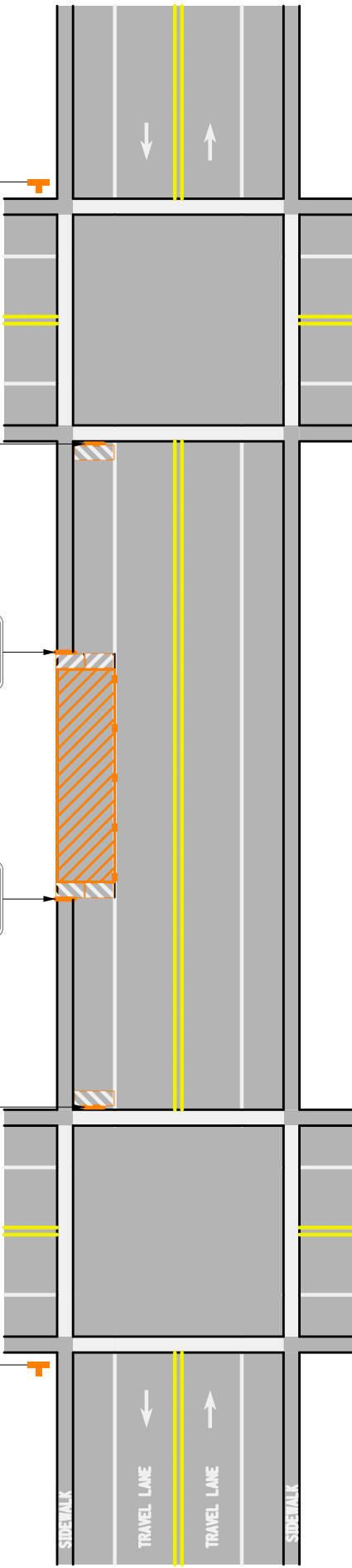
APPROVED APRIL 8, 2022

ISSUED UNDER EI 22-008

Robert Limoges
 ROBERT LIMOGES, P.E.
 DIRECTOR, OTSM

619-421

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NOT TO SCALE

SIDEWALK DETOUR

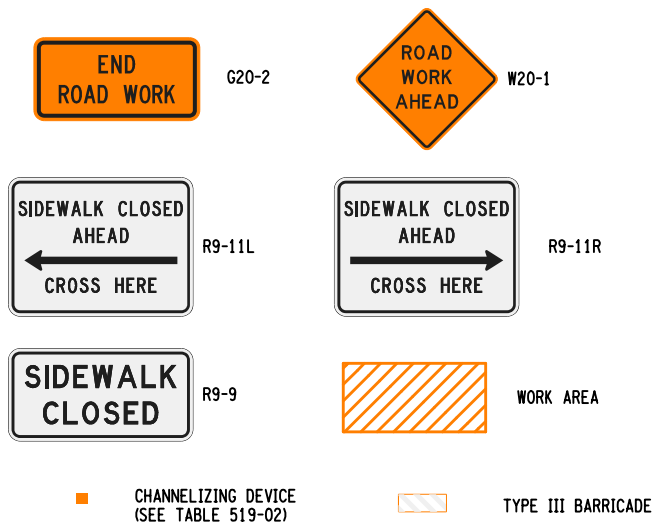


TABLE 519-01: REQUIRED SIGN SIZES*

SIGN	NON-FREEWAY	FREEWAY
G20-2	36x18	48x24
R9-11L/R9-11R**	24x18	24x18
R9-9**	24x12	24x12
W20-1	36x36	48x48

*FREEWAY SIZES MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST.
 **SIGNS NOT FOR FREEWAY USE

TABLE 519-02: CHANNELIZING DEVICE APPLICATION FOR LONG-TERM STATIONARY WORK ZONES

WORK ZONE PROVISIONS	MAXIMUM DEVICE SPACING (CENTER TO CENTER)	CHANNELIZING DEVICE									
		DRUMS	STANDARD CONES	TALL CONES	EXTRA TALL CONES	TEMPORARY TUBULAR MARKERS	INTERIM TUBULAR MARKERS	VERTICAL PANELS	OVERSIZED VERTICAL PANELS	TYPE II BARRICADES	TYPE III BARRICADES
LONG-TERM STATIONARY WORK ZONES INVOLVE WORK THAT OCCUPIES A LOCATION FOR MORE THAN 3 CONSECUTIVE DAYS.											
SHOULDER/MERGING/SHIFTING TAPERS	20 FT.	X							X	X	

NOTES: X= ALLOWED BLANK = NOT ALLOWED

NOTES:

1. LONG-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN 3 CONSECUTIVE DAYS. THIS SETUP CAN ALSO BE USED FOR INTERMEDIATE TERM WORK.
2. ANY ALTERNATIVE PEDESTRIAN PATHWAY MUST HAVE AT LEAST THE SAME LEVEL OF ACCESSIBILITY AS THE ONE IT IS REPLACING.
3. ONLY THE WORK ZONE TRAFFIC CONTROL DEVICES RELATED TO PEDESTRIANS ARE SHOWN, OTHER DEVICES, SUCH AS LANE/SHOULDER CLOSURE SIGNING, ROAD NARROWS SIGNS (W5-4), OR NO PARKING SIGNS MAY BE USED TO CONTROL VEHICULAR TRAFFIC.
4. TYPE III BARRICADES SHALL BE THE FULL WIDTH OF THE PATH BEING CLOSED.
5. ACCESS TO BUSINESS ENTRANCES AND TRANSIT STOPS WILL NEED TO BE MAINTAINED. IF THE PEDESTRIAN FACILITY CURRENTLY HAS A TRANSIT STOP THAT WILL BE AFFECTED BY THE WORK ZONE, PRACTITIONERS SHOULD CONSULT THE TRANSIT AUTHORITIES THAT SERVICE THE AFFECTED STOP TO DETERMINE HOW TO ACCOMMODATE THE STOP.

NOTES ON NIGHTTIME WORK:

- N1. WORK OCCURRING AFTER SUNSET AND BEFORE SUNRISE WILL BE CONSIDERED NIGHTTIME OPERATIONS.
- N2. ALL SIGNS, STOP/SLOW PADDLES AND RED FLAGS USED TO WARN/ALERT/CONTROL TRAFFIC SHALL BE RETROREFLECTIVE.
- N3. ALL WORKERS INVOLVED SHALL WEAR PROTECTIVE HELMETS AND NIGHTTIME APPAREL IN ACCORDANCE WITH §107-05A. HIGH VISIBILITY APPAREL AT ALL TIMES.
- N4. VEHICLES OPERATING ON THE PAVEMENT OF A CLOSED ROADWAY OR TRAVEL LANE SHALL DISPLAY ROTATING AMBER BEACONS OR FLASHING LED BEACONS AT ALL TIMES.
- N5. LEVEL I ILLUMINATION SHALL BE PROVIDED NEAR THE BEGINNING OF LANE CLOSURE TAPERS AND AT ROAD CLOSURES, INCLUDING THE SETUP AND REMOVAL OF THE CLOSURE TAPERS.
- N6. LEVEL II ILLUMINATION SHALL BE PROVIDED FOR FLAGGING STATIONS, ASPHALT PAVING, MILLING, AND CONCRETE PLACEMENT AND/OR REMOVAL OPERATIONS, INCLUDING BRIDGE DECKS, 50 FEET AHEAD OF AND 100 FEET BEHIND A PAVING OR MILLING MACHINE.
- N7. LEVEL III ILLUMINATION SHALL BE PROVIDED FOR PAVEMENT OR STRUCTURAL CRACK FILLING, JOINT REPAIR, PAVEMENT PATCHING AND REPAIRS, INSTALLATION OF SIGNAL EQUIPMENT OR OTHER ELECTRICAL/MECHANICAL EQUIPMENT, AND OTHER TASKS INVOLVING FINE DETAILS OR INTRICATE PARTS AND EQUIPMENT.
- N8. ALL LIGHTING SHALL BE DESIGNED, INSTALLED, AND OPERATED TO AVOID GLARE THAT AFFECTS TRAFFIC ON THE ROADWAY OR THAT CAUSES ANNOYANCE OR DISCOMFORT FOR RESIDENCES ADJOINING THE ROADWAY.
- N9. PRIOR TO THE START OF NIGHTTIME OPERATIONS, A WRITTEN NIGHTTIME OPERATIONS AND LIGHTING PLAN IS REQUIRED FOR APPROVAL.
- N10. AT NIGHT, EACH TYPE III CONSTRUCTION BARRICADE USED TO CLOSE A ROADWAY, A SEGMENT OF A ROADWAY OR A SIDEWALK SHALL BE EQUIPPED WITH ONE FLASHING WARNING LIGHT.
- N11. SEE NIGHTTIME SAFETY BULLETIN, HDM §16.5.7, & STANDARD SPECIFICATIONS §619 FOR ADDITIONAL REQUIREMENTS AND CONSIDERATIONS.

NEW YORK
STATE OF OPPORTUNITY.

Department of Transportation

U.S. CUSTOMARY STANDARD SHEET

**WORK ZONE TRAFFIC CONTROL
TWO-LANE TWO-WAY ROADWAY
SIDEWALK DETOUR/DIVERSION
LONG TERM OPERATION
(SHEET 1 OF 2)**

APPROVED DECEMBER 2, 2021
Robert Limoges
ROBERT LIMOGES, P.E.
DIRECTOR, OTSM

ISSUED UNDER EI 21-028
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ACCESSIBILITY RAMP (SEE NOTE 6)

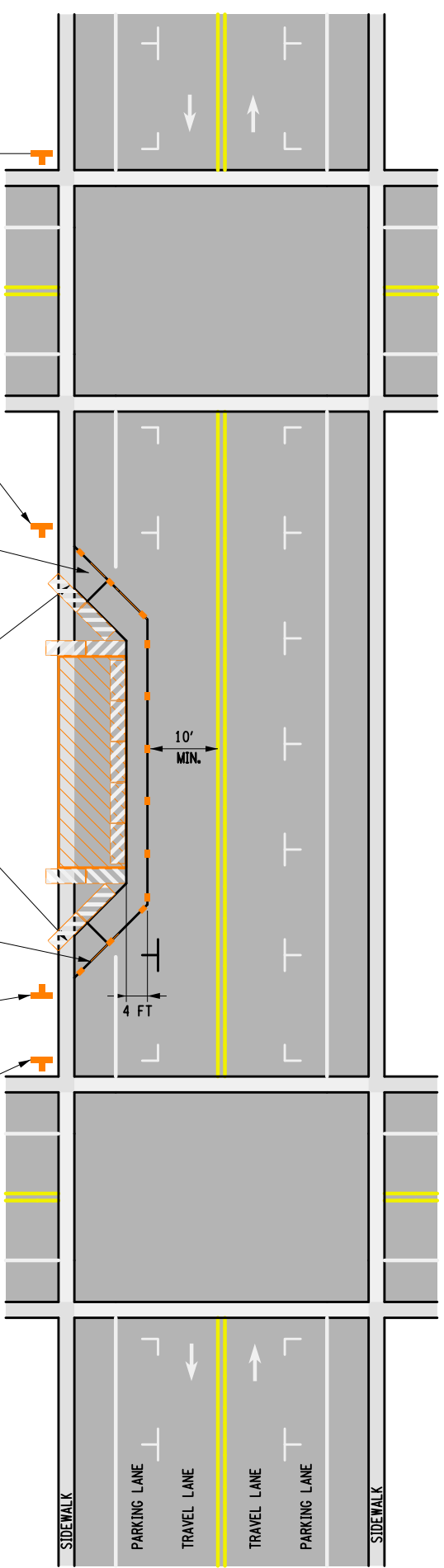
SIDEWALK CLOSED

SIDEWALK CLOSED

ACCESSIBILITY RAMP (SEE NOTE 6)



END ROAD WORK



DETAIL 519A: SIDEWALK DIVERSION
 - POSTED SPEED LIMIT < 45 MPH



ACCESSIBILITY RAMP (SEE NOTE 6)

SIDEWALK CLOSED

SIDEWALK CLOSED

ACCESSIBILITY RAMP (SEE NOTE 6)



END ROAD WORK

TAPERED END SECTION OR TEMPORARY IMPACT ATTENUATOR

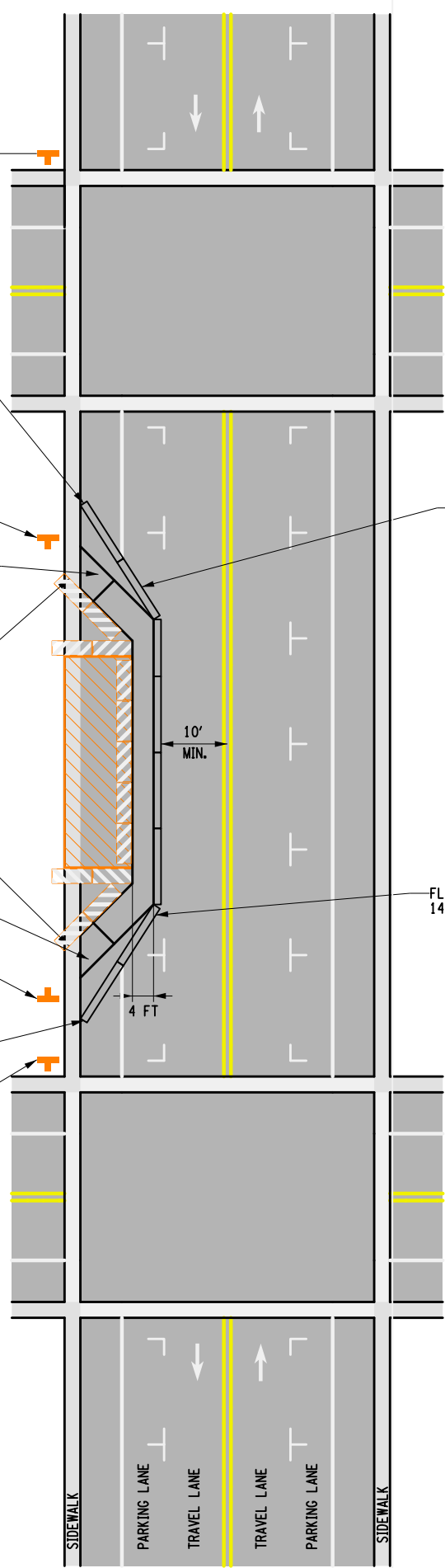
TAPERED END SECTION OR TEMPORARY IMPACT ATTENUATOR

FLARE RATE 14:1 TO 16:1

FLARE RATE 14:1 TO 16:1

10' MIN.

4 FT



DETAIL 519B: SIDEWALK DIVERSION
 - POSTED SPEED LIMIT ≥ 45 MPH

NOTES:

- LONG-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN 3 CONSECUTIVE DAYS. THIS SETUP CAN ALSO BE USED FOR INTERMEDIATE TERM WORK.
- ANY SIDEWALK DIVERSION MUST BE ADA COMPLIANT AND FOLLOW THE 2011 PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG). TEMPORARY RAMPS AND OTHER TEMPORARY PEDESTRIAN FACILITIES SHALL BE PAID FOR UNDER THE APPROPRIATE ITEM FOR HOT MIX ASPHALT (HMA) SIDEWALKS.
- PROWAG SECTION R205 AND MUTCD PART 6 SHALL APPLY TO ALL TEMPORARY ALTERNATIVE PEDESTRIAN PATHWAYS.
- ONLY THE WORK ZONE TRAFFIC CONTROL DEVICES RELATED TO PEDESTRIANS ARE SHOWN. OTHER DEVICES, SUCH AS LANE/SHOULDER CLOSURE SIGNING, ROAD NARROWS SIGNS (W5-4), OR NO PARKING SIGNS MAY BE USED TO CONTROL VEHICULAR TRAFFIC.
- TYPE III BARRICADES SHALL BE THE FULL WIDTH OF THE PATH BEING CLOSED.
- THE GRADE (RUNNING SLOPE) OF A TEMPORARY CURB RAMP FOR DESIGN AND LAYOUT SHALL BE A MAXIMUM OF 7.5%. THE GRADE FOR ADA ACCESSIBILITY AND WORK ACCEPTANCE SHALL BE A MAXIMUM OF 8.3%.
- ACCESS TO BUSINESS ENTRANCES AND TRANSIT STOPS WILL NEED TO BE MAINTAINED. IF THE PEDESTRIAN FACILITY CURRENTLY HAS A TRANSIT STOP THAT WILL BE AFFECTED BY THE WORK ZONE, PRACTITIONERS SHOULD CONSULT THE TRANSIT AUTHORITIES THAT SERVICE THE AFFECTED STOP TO DETERMINE HOW TO ACCOMMODATE THE STOP.

NOTES ON NIGHTTIME WORK:

- WORK OCCURRING AFTER SUNSET AND BEFORE SUNRISE WILL BE CONSIDERED NIGHTTIME OPERATIONS.
- ALL SIGNS, STOP/SLOW PADDLES AND RED FLAGS USED TO WARN/ALERT/CONTROL TRAFFIC SHALL BE RETROREFLECTIVE.
- ALL WORKERS INVOLVED SHALL WEAR PROTECTIVE HELMETS AND NIGHTTIME APPAREL IN ACCORDANCE WITH §107-05A. HIGH VISIBILITY APPAREL AT ALL TIMES.
- VEHICLES OPERATING ON THE PAVEMENT OF A CLOSED ROADWAY OR TRAVEL LANE SHALL DISPLAY ROTATING AMBER BEACONS OR FLASHING LED BEACONS AT ALL TIMES.
- LEVEL I ILLUMINATION SHALL BE PROVIDED NEAR THE BEGINNING OF LANE CLOSURE TAPERS AND AT ROAD CLOSURES, INCLUDING THE SETUP AND REMOVAL OF THE CLOSURE TAPERS.
- LEVEL II ILLUMINATION SHALL BE PROVIDED FOR FLAGGING STATIONS, ASPHALT PAVING, MILLING, AND CONCRETE PLACEMENT AND/OR REMOVAL OPERATIONS, INCLUDING BRIDGE DECKS, 50 FEET AHEAD OF AND 100 FEET BEHIND A PAVING OR MILLING MACHINE.
- LEVEL III ILLUMINATION SHALL BE PROVIDED FOR PAVEMENT OR STRUCTURAL CRACK FILLING, JOINT REPAIR, PAVEMENT PATCHING AND REPAIRS, INSTALLATION OF SIGNAL EQUIPMENT OR OTHER ELECTRICAL/MECHANICAL EQUIPMENT, AND OTHER TASKS INVOLVING FINE DETAILS OR INTRICATE PARTS AND EQUIPMENT.
- ALL LIGHTING SHALL BE DESIGNED, INSTALLED, AND OPERATED TO AVOID GLARE THAT AFFECTS TRAFFIC ON THE ROADWAY OR THAT CAUSES ANNOYANCE OR DISCOMFORT FOR RESIDENCES ADJOINING THE ROADWAY.
- PRIOR TO THE START OF NIGHTTIME OPERATIONS, A WRITTEN NIGHTTIME OPERATIONS AND LIGHTING PLAN IS REQUIRED FOR APPROVAL.
- AT NIGHT, EACH TYPE III CONSTRUCTION BARRICADE USED TO CLOSE A ROADWAY, A SEGMENT OF A ROADWAY OR A SIDEWALK SHALL BE EQUIPPED WITH ONE FLASHING WARNING LIGHT.
- SEE NIGHTTIME SAFETY BULLETIN, HDM §16.5.7, & STANDARD SPECIFICATIONS §619 FOR ADDITIONAL REQUIREMENTS AND CONSIDERATIONS.



THIS SIGN SHALL BE LOCATED A MINIMUM DISTANCE OF 80 FT. AND MAXIMUM OF 400 FT. PAST THE END OF THE DOWNSTREAM TAPER



R9-9



W20-1



M4-9bL



M4-9bR



TYPE III BARRICADE



PINNED TEMPORARY POSITIVE BARRIER



CHANNELIZING DEVICE (SEE TABLE 519-02 ON SHEET 1 OF 2)



WORK AREA

TABLE 519-03: REQUIRED SIGN SIZES*

SIGN	NON-FREEWAY	FREEWAY
G20-2	36x18	48x24
M4-9bL/M4-9bR**	30x24	30x24
R9-9**	24x12	24x12
W20-1	36x36	48x48

*FREEWAY SIZES MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST.
 **SIGNS NOT FOR FREEWAY USE.



U.S. CUSTOMARY STANDARD SHEET

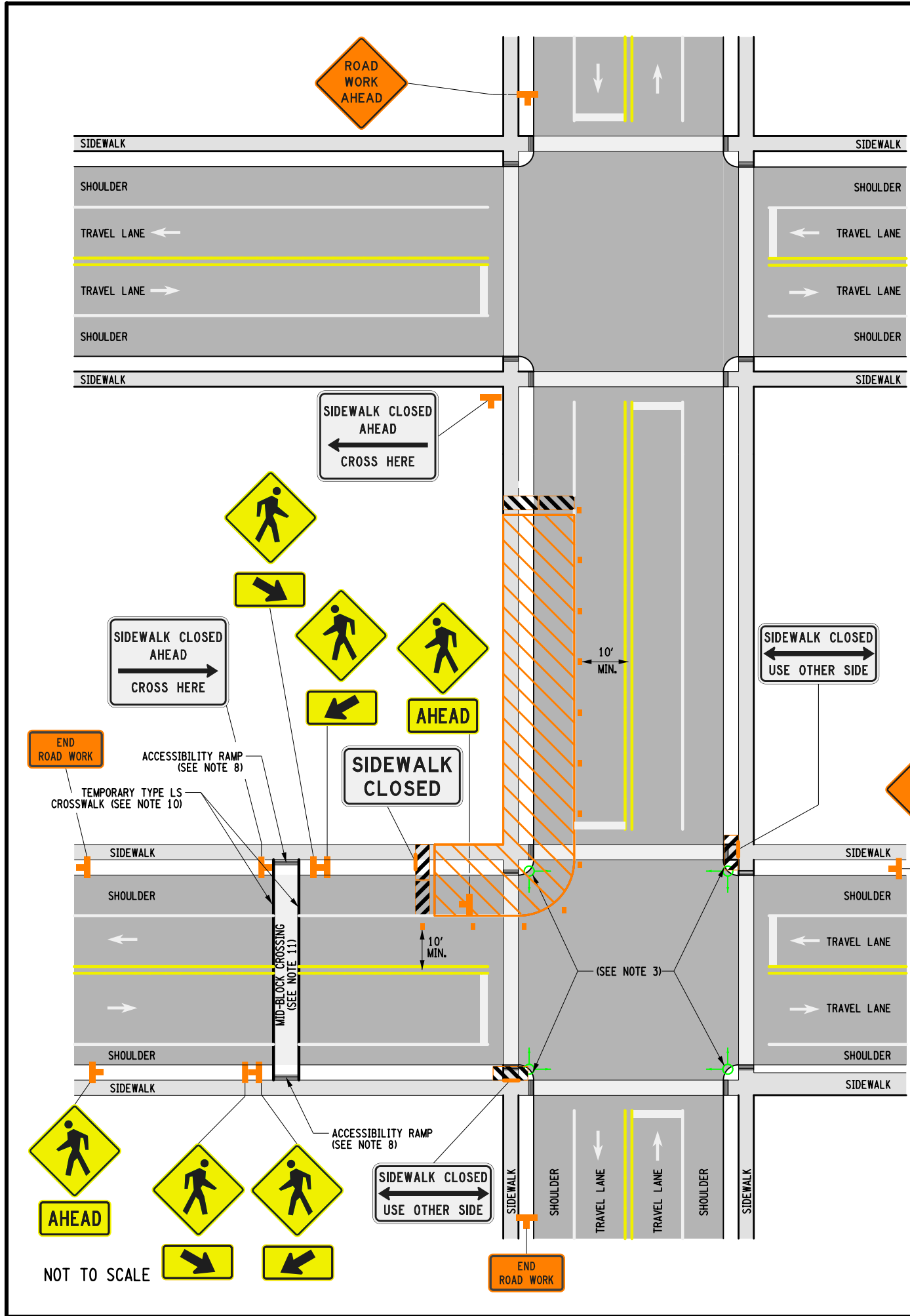
WORK ZONE TRAFFIC CONTROL
 TWO-LANE TWO-WAY ROADWAY
 SIDEWALK DETOUR/DIVERSION
 LONG TERM OPERATION
 (SHEET 2 OF 2)

APPROVED DECEMBER 2, 2021
 Robert Limoges
 ROBERT LIMOGES, P.E.
 DIRECTOR, OTSM

ISSUED UNDER EI 21-028
 619-519

NOT TO SCALE

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END ROAD WORK G20-2 THIS SIGN SHALL BE LOCATED A MINIMUM DISTANCE OF 80 FT. AND MAXIMUM OF 400 FT. PAST THE END OF THE DOWNSTREAM TAPER

AHEAD W16-9P

ROAD WORK AHEAD W20-1

W16-7PR W16-7PL

SIDEWALK CLOSED AHEAD CROSS HERE R9-11L

W11-2L W11-2L

SIDEWALK CLOSED R9-9

R8-3 (SEE NOTE 2)

SIDEWALK CLOSED AHEAD CROSS HERE R9-11R

SIDEWALK CLOSED USE OTHER SIDE R9-10

CHANNELIZING DEVICE (SEE TABLE 520-02)

WORK AREA

TYPE III BARRICADE

NOT TO SCALE

REFER TO SHEET 2 OF 2 FOR ALL TABLES

NEW YORK STATE OF OPPORTUNITY.		Department of Transportation
U.S. CUSTOMARY STANDARD SHEET		
WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY ROADWAY CROSSWALK CLOSURE AND PEDESTRIAN DETOUR LONG TERM OPERATION (SHEET 1 OF 2)		
APPROVED DECEMBER 2, 2021	ISSUED UNDER EI 21-028	
<i>Robert Limoges</i> ROBERT LIMOGES, P.E. DIRECTOR, OTSM	619-520	

TABLE 520-01: REQUIRED SIGN SIZES*

SIGN	NON-FREEWAY	FREEWAY
G20-2	36x18	48x24
R8-3	24x24	48x48
R9-9**	24x12	24x12
R9-10**	24x12	24x12
R9-11L/R9-11R**	24x18	24x18
W11-2L/W11-2R	48x30	48x30
W16-9P**	24x12	30x18
W16-7PL/W16-7PR**	24x12	30x18

*FREEWAY SIZES MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST.
**SIGNS NOT FOR FREEWAY USE.

TABLE 520-02: CHANNELIZING DEVICE APPLICATION FOR LONG-TERM STATIONARY WORK ZONES

WORK ZONE PROVISIONS	MAXIMUM DEVICE SPACING (CENTER TO CENTER)	MUTCD COMPLIANT CHANNELIZING DEVICE								
		DRUMS	STANDARD CONES	TALL CONES	EXTRA TALL CONES	TEMPORARY TUBULAR MARKERS	INTERIM TUBULAR MARKERS	VERTICAL PANELS	OVERSIZED VERTICAL PANELS	TYPE III BARRICADES
LONG-TERM STATIONARY WORK ZONES INVOLVE WORK THAT OCCUPIES A LOCATION FOR MORE THAN 3 CONSECUTIVE DAYS.	20 FT.	X							X	

NOTES: X= ALLOWED, BLANK = NOT ALLOWED

TABLE 520-03: GUIDELINES FOR ADVANCE PLACEMENT OF WARNING SIGNS

POSTED OR 85 TH PERCENTILE SPEED (MPH)	CONDITION A: SPEED REDUCTION AND LANE CHANGING IN HEAVY TRAFFIC ²	ADVANCE PLACEMENT DISTANCE (FT.) ¹											
		CONDITION B: DECELERATION TO THE LISTED ADVISORY SPEED (MPH) FOR THE CONDITION ⁴											
		CONDITION C: NO SPEED REDUCTION NECESSARY ⁵											
		0 ³	5	10	15	20	25	30	35	40	45	50	55
20	410	115	110	105	90	75	-	-	-	-	-	-	-
25	515	155	160	150	135	120	95	-	-	-	-	-	-
30	620	200	205	195	185	165	140	110	-	-	-	-	-
35	720	250	255	245	235	215	190	160	130	-	-	-	-
40	825	305	320	310	295	280	255	225	190	150	-	-	-
45	930	360	380	370	360	340	315	285	255	210	165	-	-
50	1030	425	455	450	435	415	390	360	330	285	240	185	-
55	1135	495	530	520	505	490	460	435	400	355	315	255	205

NOTES:


- THE DISTANCES HAVE NOT BEEN MODIFIED TO ACCOUNT FOR SIGN LEGIBILITY.
- TYPICAL CONDITIONS ARE LOCATIONS WHERE THE ROAD USER MUST USE EXTRA TIME TO ADJUST SPEED AND CHANGE LANES IN HEAVY TRAFFIC BECAUSE OF A COMPLEX DRIVING SITUATION. TYPICAL SIGNS ARE MERGE AND RIGHT LAND ENDS. THE DISTANCES ARE TAKEN FROM THE 2004 AASHTO POLICY, EXHIBIT 3-3, DECISION SIGHT DISTANCE, AVOIDANCE MANEUVER E.
- TYPICAL CONDITION IS THE WARNING OF A POTENTIAL STOP SITUATION. TYPICAL SIGNS ARE STOP AHEAD, YIELD AHEAD, SIGNAL AHEAD, AND INTERSECTION WARNING SIGNS.
- TYPICAL CONDITIONS ARE LOCATIONS WHERE THE ROAD USER MUST DECREASE SPEED TO MANEUVER THROUGH THE WARNED CONDITION. TYPICAL SIGNS ARE TURN, CURVE, REVERSE TURN, OR REVERSE CURVE. THE DISTANCES ARE DETERMINED BY PROVIDING A 2.5 SECOND PRT AND A VEHICLE DECELERATION RATE OF 10 FT./SECOND.²
- TYPICAL SIGNS ARE ADDED LANE, ROAD NARROWS, DIVIDED HIGHWAY, AND CONDITION B SIGNS DISPLAYED WITHOUT AN ADVISORY SPEED.

NOTES:

- LONG-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN 3 CONSECUTIVE DAYS. THIS SETUP CAN ALSO BE USED FOR INTERMEDIATE TERM WORK.
- PARKING ALONG THE CURB SHALL BE PROHIBITED FOR AT LEAST 50' IN ADVANCE OF THE MID-BLOCK CROSSWALK. IF THE SHOULDER IS GREATER THAN OR EQUAL TO 8 FOOT, AN R8-3 SIGN IS RECOMMENDED TO PREVENT PARKING ON SHOULDER.
- PEDESTRIAN TRAFFIC SIGNAL DISPLAYS CONTROLLING CLOSED CROSSWALKS SHOULD BE COVERED OR DEACTIVATED.
- ONLY THE WORK ZONE TRAFFIC CONTROL DEVICES RELATED TO PEDESTRIANS ARE SHOWN. OTHER DEVICES, SUCH AS LANE/SHOULDER CLOSURE SIGNING, ROAD NARROWS SIGNS (W5-4), OR NO PARKING SIGNS MAY BE USED TO CONTROL VEHICULAR TRAFFIC.
- ANY FEATURES CONSTRUCTED AS PART OF A PEDESTRIAN DETOUR MUST BE ADA COMPLIANT AND FOLLOW THE 2011 PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG). TEMPORARY RAMPS AND OTHER TEMPORARY PEDESTRIAN FACILITIES SHALL BE PAID FOR UNDER THE APPROPRIATE ITEM FOR HOT MIX ASPHALT (HMA) SIDEWALKS.
- PROWAG SECTION R205 AND MUTCD PART 6 SHALL APPLY TO ALL TEMPORARY ALTERNATIVE PEDESTRIAN PATHWAYS.
- TYPE III BARRICADES SHALL BE THE FULL WIDTH OF THE PATH BEING CLOSED.
- THE GRADE (RUNNING SLOPE), AND CROSS SLOPE OF A TEMPORARY CURB RAMP FOR DESIGN AND LAYOUT SHALL BE IN ACCORDANCE WITH STANDARD SHEET 608-01.
- ACCESS TO BUSINESS ENTRANCES AND TRANSIT STOPS WILL NEED TO BE MAINTAINED. IF THE PEDESTRIAN FACILITY CURRENTLY HAS A TRANSIT STOP THAT WILL BE AFFECTED BY THE WORK ZONE, PRACTITIONERS SHOULD CONSULT THE TRANSIT AUTHORITIES THAT SERVICE THE AFFECTED STOP TO DETERMINE HOW TO ACCOMMODATE THE STOP.
- FOR LONG-TERM STATIONARY WORK, THE DOUBLE YELLOW CENTERLINE AND/OR LANE LINES SHOULD BE REMOVED BETWEEN THE TEMPORARY CROSSWALK LINES.
- ALL MID BLOCK CROSSWALKS SHALL BE APPROVED BY THE DOT REGIONAL DIRECTOR OR HIS/HER DESIGNEE.

NOTES ON NIGHTTIME WORK:

- WORK OCCURRING AFTER SUNSET AND BEFORE SUNRISE WILL BE CONSIDERED NIGHTTIME OPERATIONS.
- ALL SIGNS, STOP/SLOW PADDLES AND RED FLAGS USED TO WARN/ALERT/CONTROL TRAFFIC SHALL BE RETROREFLECTIVE.
- ALL WORKERS INVOLVED SHALL WEAR PROTECTIVE HELMETS AND NIGHTTIME APPAREL IN ACCORDANCE WITH §107-05A. HIGH VISIBILITY APPAREL AT ALL TIMES.
- VEHICLES OPERATING ON THE PAVEMENT OF A CLOSED ROADWAY OR TRAVEL LANE SHALL DISPLAY ROTATING AMBER BEACONS OR FLASHING LED BEACONS AT ALL TIMES.
- LEVEL I ILLUMINATION SHALL BE PROVIDED NEAR THE BEGINNING OF LANE CLOSURE TAPERS AND AT ROAD CLOSURES, INCLUDING THE SETUP AND REMOVAL OF THE CLOSURE TAPERS.
- LEVEL II ILLUMINATION SHALL BE PROVIDED FOR FLAGGING STATIONS, ASPHALT PAVING, MILLING, AND CONCRETE PLACEMENT AND/OR REMOVAL OPERATIONS, INCLUDING BRIDGE DECKS, 50 FEET AHEAD OF AND 100 FEET BEHIND A PAVING OR MILLING MACHINE.
- LEVEL III ILLUMINATION SHALL BE PROVIDED FOR PAVEMENT OR STRUCTURAL CRACK FILLING, JOINT REPAIR, PAVEMENT PATCHING AND REPAIRS, INSTALLATION OF SIGNAL EQUIPMENT OR OTHER ELECTRICAL/MECHANICAL EQUIPMENT, AND OTHER TASKS INVOLVING FINE DETAILS OR INTRICATE PARTS AND EQUIPMENT.
- ALL LIGHTING SHALL BE DESIGNED, INSTALLED, AND OPERATED TO AVOID GLARE THAT AFFECTS TRAFFIC ON THE ROADWAY OR THAT CAUSES ANNOYANCE OR DISCOMFORT FOR RESIDENCES ADJOINING THE ROADWAY.
- PRIOR TO THE START OF NIGHTTIME OPERATIONS, A WRITTEN NIGHTTIME OPERATIONS AND LIGHTING PLAN IS REQUIRED FOR APPROVAL FROM THE DOT ENGINEER.
- AT NIGHT, EACH TYPE III CONSTRUCTION BARRICADE USED TO CLOSE A ROADWAY, A SEGMENT OF A ROADWAY OR A SIDEWALK SHALL BE EQUIPPED WITH ONE FLASHING WARNING LIGHT.
- SEE STANDARD SPECIFICATIONS §619 FOR ADDITIONAL REQUIREMENTS AND CONSIDERATIONS.

 NEW YORK STATE OF OPPORTUNITY.	Department of Transportation
U.S. CUSTOMARY STANDARD SHEET	
WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY ROADWAY CROSSWALK CLOSURE AND PEDESTRIAN DETOUR LONG TERM OPERATION (SHEET 2 OF 2)	
APPROVED APRIL 8, 2022	ISSUED UNDER EI 22-008
<i>Robert Limoges</i> ROBERT LIMOGES, P.E. DIRECTOR, OTSM	619-520